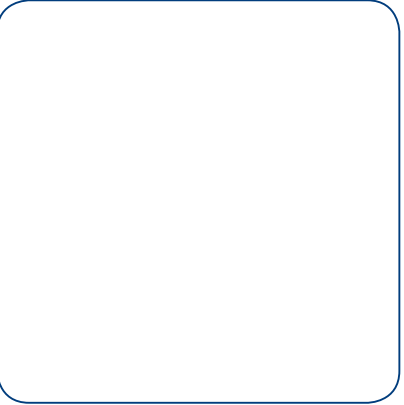
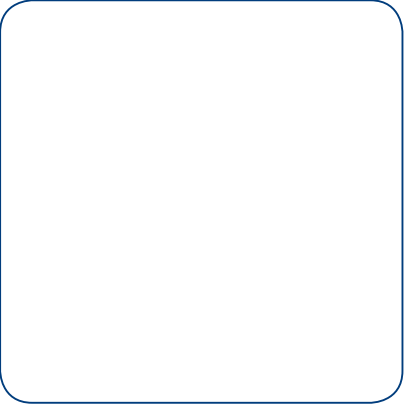
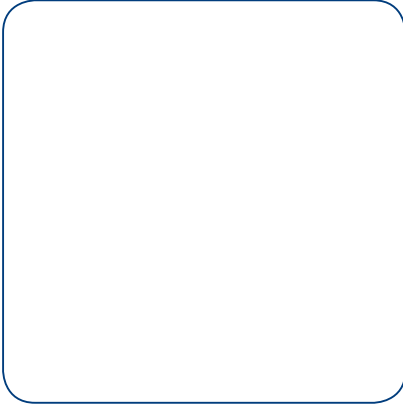
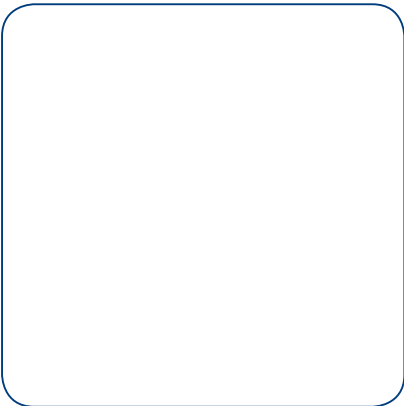


# Alexandra Basin Redevelopment Project

## Environmental Impact Statement

Volume 3 | Appendices



APPENDIX 6 LANDSCAPE & VISUAL

**Preamble:**

- This Appendix should be read in conjunction with Chapter 6 of the EIS, Landscape and Visual and associated figures, including 6.2 Photomontage Viewpoint Locations.
- As outlined within Chapter 6 of the EIS, a series of representative viewpoints have been selected from locations within the development's Zone of Visual Influence (ZVI) and subjected to specific assessment.
- For a number of the viewpoints selected, photomontages have been provided to demonstrate the visual impact from the particular viewpoint; these photomontages are representative of a 40 degree field of view (when viewed at arms length and printed at A3 [297mmx420mm]).
- In addition to photomontages, panoramic views have also been provided (to an increased field of view 100 degrees), to demonstrate the wider context of the development to the reader. In a number of instances where the development is obscured by topography or existing structures, a wireframe image has been provided to indicate the placement of the development in the view.
- 4 viewpoints (1, 2, 12 and 13) have been provided with photographs only due to the fact that distance and / or intervening topography rules out any view of the development from these locations.



**Viewpoint 1 St Fintans Burial Ground, Howth – Photograph Only**



## Viewpoint 2 Sutton Strand – Photograph Only





**Viewpoint 3 Bull Island Visitors Centre – Existing Photograph**



Viewpoint 3 Bull Island Visitors Centre – Photomontage





### Viewpoint 3 Bull Island Visitors Centre – Existing Photograph Panorama





### Viewpoint 3 Bull Island Visitors Centre – Photomontage Panorama





### Viewpoint 3 Bull Island Visitors Centre – Wireframe Panorama





Viewpoint 4 St Anne’s Park, Clontarf – Existing Photograph





Viewpoint 4 St Anne’s Park, Clontarf – Photomontage





**Viewpoint 4 St Anne's Park, Clontarf – Existing Photograph Panorama**



**Viewpoint 4 St Anne's Park, Clontarf – Photomontage Panorama**



**Viewpoint 4 St Anne's Park, Clontarf – Wireframe Panorama**



## Viewpoint 5 Bull Wall – Existing Photograph





## Viewpoint 5 Bull Wall – Photomontage





## Viewpoint 5 Bull Wall – Existing Photograph Panorama





## Viewpoint 5 Bull Wall – Photomontage Panorama





## Viewpoint 5 Bull Wall – Wireframe Panorama





**Viewpoint 6 Clontarf Car Park – Existing Photograph**



## Viewpoint 6 Clontarf Car Park – Photomontage





**Viewpoint 6 Clontarf Car Park – Existing Photograph Panorama**



**Viewpoint 6 Clontarf Car Park – Photomontage Panorama**



**Viewpoint 6 Clontarf Car Park – Wireframe Panorama**



**Viewpoint 7 Fairview Park – Existing Photograph**



**Viewpoint 7 Fairview Park – Photomontage**



## Viewpoint 7 Fairview Park – Existing Photograph Panorama





## Viewpoint 7 Fairview Park – Photomontage Panorama





## Viewpoint 7 Fairview Park – Wireframe Panorama





Viewpoint 8 North Wall Quay – Existing Photograph





Viewpoint 8 North Wall Quay –Photomontage





**Viewpoint 8 North Wall Quay – Existing Photograph Panorama**



Viewpoint 8 North Wall Quay – Photomontage Panorama



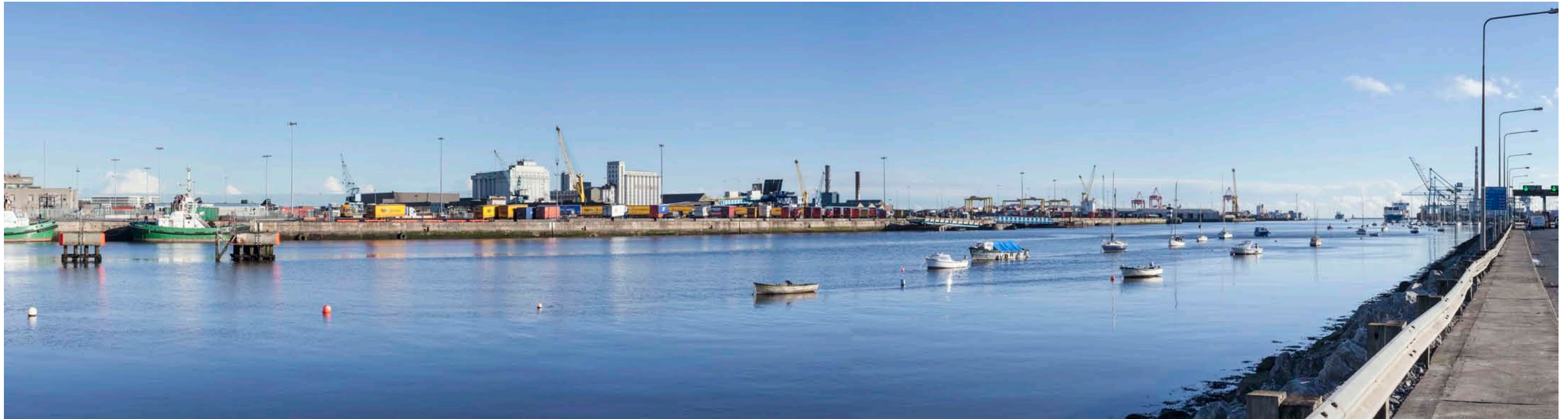


**Viewpoint 9 East Link Toll – Existing Photograph**



**Viewpoint 9 East Link Toll – Photomontage**



**Viewpoint 9 East Link Toll – Existing Photograph Panorama**



## Viewpoint 9 East Link Toll – Photomontage Panorama





Viewpoint 10 Sandymount Strand Car Park – Existing Photograph





**Viewpoint 10 Sandymount Strand Car Park – Photomontage**



**Viewpoint 10 Sandymount Strand Car Park – Existing Photograph Panorama**



**Viewpoint 10 Sandymount Strand Car Park – Photomontage Panorama**



**Viewpoint 10 Sandymount Strand Car Park – Wireframe Panorama**



Viewpoint 11 Clontarf Road Promenade – Existing Photograph





Viewpoint 11 Clontarf Road Promenade – Photomontage





## Viewpoint 11 Clontarf Road Promenade – Existing Photograph Panorama





## Viewpoint 11 Clontarf Road Promenade – Photomontage Panorama





## Viewpoint 11 Clontarf Road Promenade – Wireframe Panorama





## Viewpoint 12 Idrone Terrace, Blackrock – Photograph Only





## Viewpoint 13 Killiney Hill – Photograph Only





APPENDIX 8 MATERIAL ASSETS



## Appendix 8.1 NRA Letter of 31 May 2011 to DPC



**Mr. Eamonn O'Reilly**  
Chief Executive  
Dublin Port Company  
Port Centre  
Alexandra Road  
Dublin 1

Teach Naomh Máirtín / Bóthar Waterloo / Baile Átha Cliath 4  
St. Martin's House / Waterloo Road / Dublin 4  
Teil: / Tel: + 353 1 660 2511 Facs: / Fax: + 353 1 668 0009

Dáta | Date 31<sup>st</sup> May 2011 Áir dTag. | Our Ref. NRA11-82336 Bhur dTag. | Your Ref.

**Re: Dublin Port Masterplan**

**Dear Mr. O'Reilly**

I refer to your correspondence concerning the above and wish to state that the Authority supports the preparation of a Masterplan for the development of Dublin Port and welcomes the engagement that has already been undertaken by Dublin Port Company with the Authority. In the light of these engagements, the Authority offers the following comments for consideration during the draft of the Masterplan.

**Protection of the Dublin Port Tunnel Asset.**

The Dublin Port Tunnel, which opened in 2006, has been provided by the Authority in conjunction with Dublin City Council in line with the Government policy to improve access to/from Dublin Port by HGVs. The policy concerned recognised the significance of the Port to the national economy and sought to combine the benefits of more efficient and safer access to Port related activities with traffic management initiatives in the wider Dublin area, in particular, Dublin City Centre. The Port Tunnel is integrated into the national road network via the M50 and the radial routes connection the motorway to the regions. In addition, the existence of the Tunnel has enabled Dublin City Council to introduce a HGV management strategy within the City Centre with significant benefits for safety, the environment and business activity generally in the area.

The Authority would welcome acknowledgement within the proposed Masterplan of the presence and strategic role of the Port Tunnel for the future operation of Dublin Port and as regards the other considerations already mentioned relating to the wider Dublin area and national transport management considerations. Such acknowledgement should be supplemented by a commitment to ensure that the structural integrity, stability, safety and operation of the Dublin Port Tunnel will continue to be protected and maintained in the context of the consideration of development proposals within the Port which, because of their nature, location or proximity to the Tunnel, could potentially have implications in the areas mentioned.

The Authority would welcome early consultation by the Port Company should development proposals be brought forward at any stage which could have structural, safety or operational implications for the Port Tunnel.

**Dublin Eastern Bypass.**

As the Dublin Port Company is aware, the Authority, in response to a Government request, has carried out a Feasibility Study into the development of a Dublin Eastern Bypass. In November, 2007, our report was forwarded to the Minister for Transport for Government consideration and decision. The Authority awaits further direction on the matter.

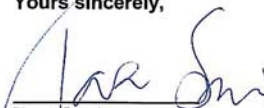
Key strategic objectives of the Eastern Bypass, would be to provide national road connectivity to service the expansion and redevelopment of the Poolbeg peninsula / South Port area, as well as the access to the Dublin Port area, and also a north-south link on the eastern side of Dublin City which would benefit traffic movement and management in the case of national and non-national roads in the area.

Copies of the Authority's Eastern Bypass Feasibility Study and of the Bypass Corridor Protection Study are enclosed for information. You might note the stage to which planning for such a Bypass, including possible junction arrangements, has advanced.

We appreciate that the design of the bypass and junction would have to be developed co-operatively with that of other developments in the area, and pending clarification of the Government's intentions on the Bypass, the Authority recommends that Masterplan policies and actions for the development of Dublin Port are compatible with construction of the Bypass at a future date should the Authority be so directed. We would welcome ongoing cooperation and consultation on aspects of the preparation and implementation of the Port Masterplan which could impact on the feasibility and operation of such a Bypass.

The Authority would be happy to discuss any issues that may arise in relation to the foregoing in the context of (a) your preparation of the Masterplan, or (b) Port operations more generally.

Yours sincerely,



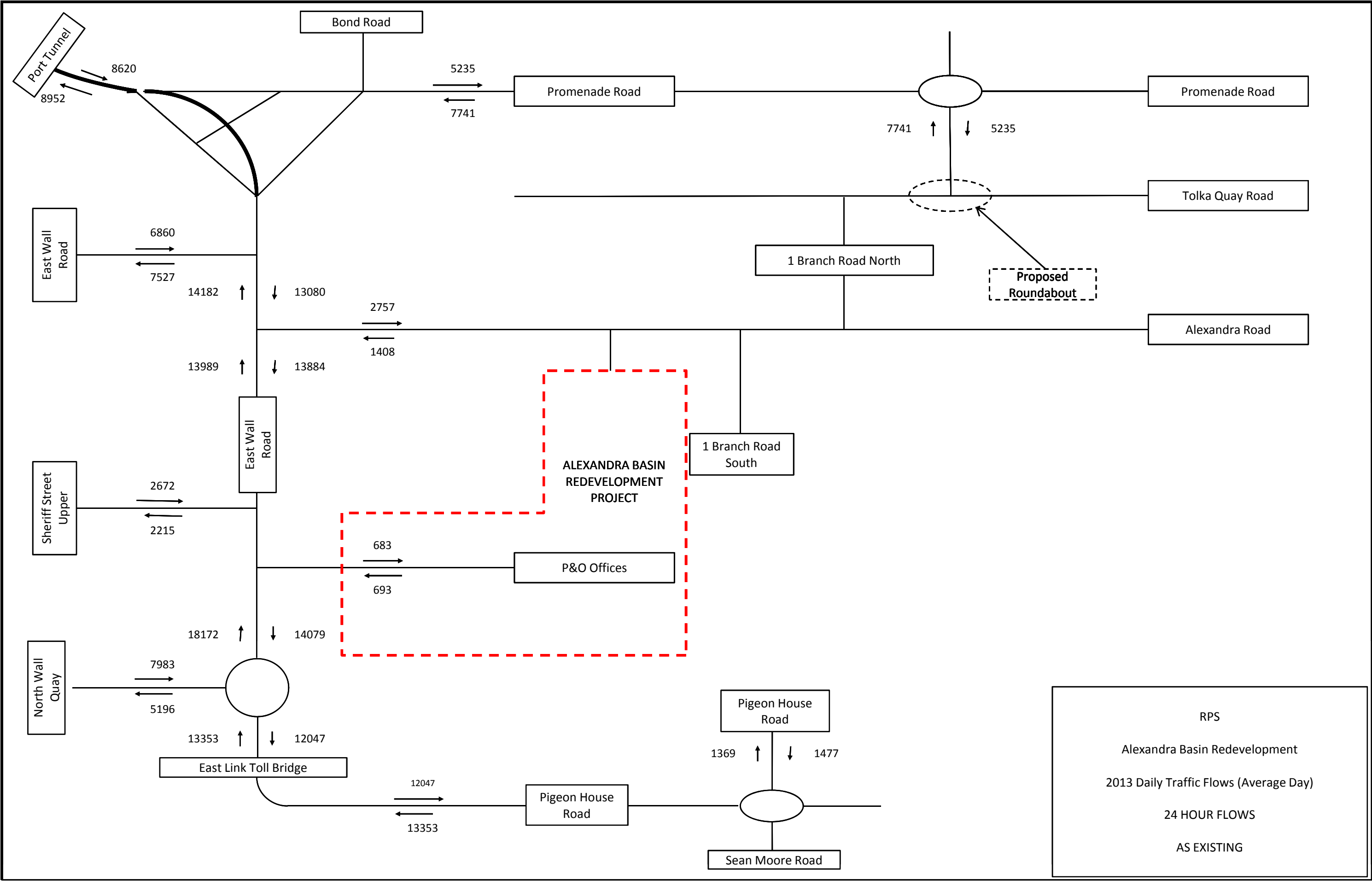
**Tara Spain,**  
Senior Policy Advisor (Planning)



**Appendix 8.2 Daily Traffic Flows (2013)**



2013 Existing Daily Traffic Flows

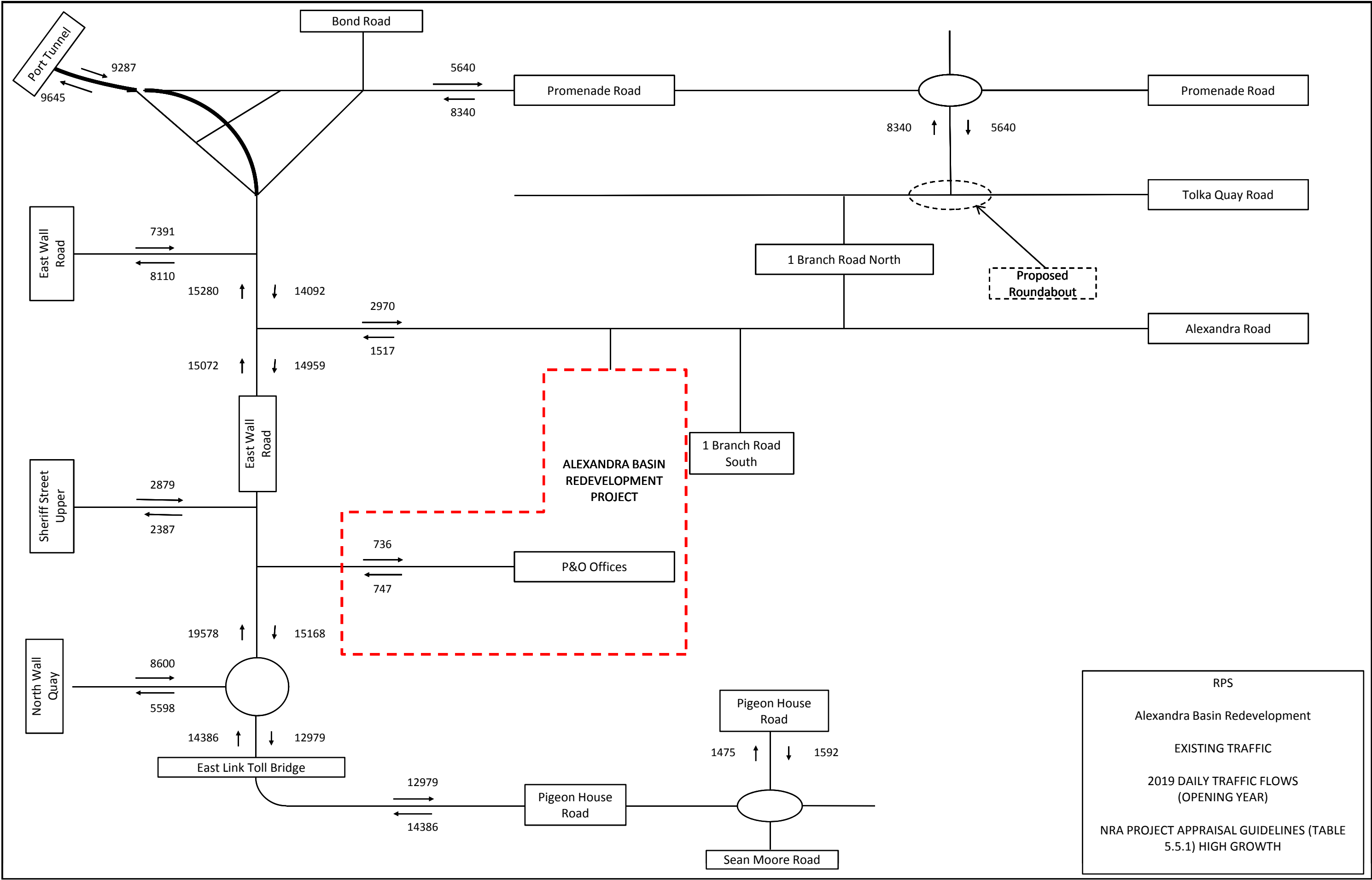




**Appendix 8.3 Predicted Daily Traffic Flows for 2019, 2014, 2034, 2040**

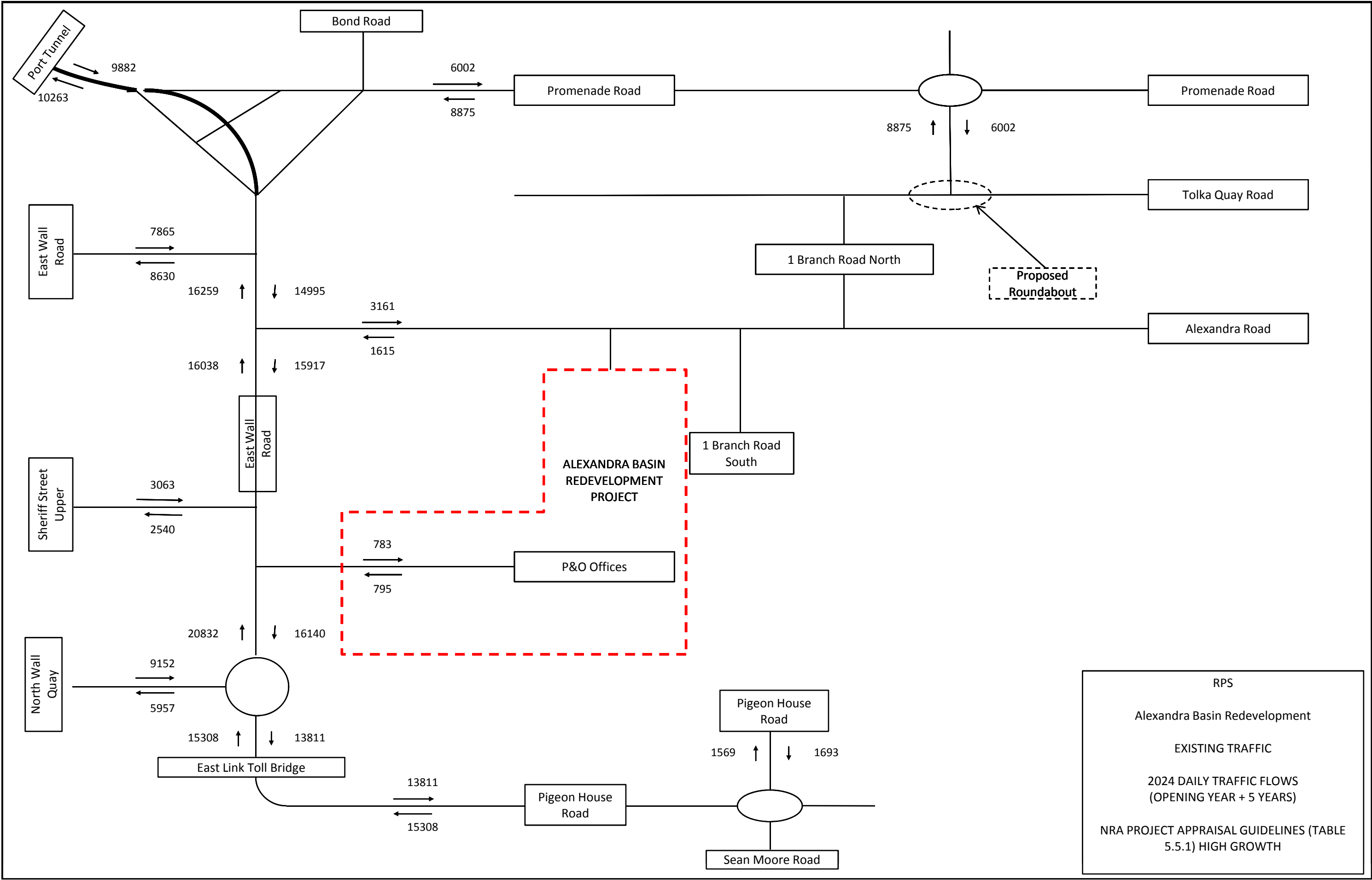


2019 Existing Daily Traffic Flows



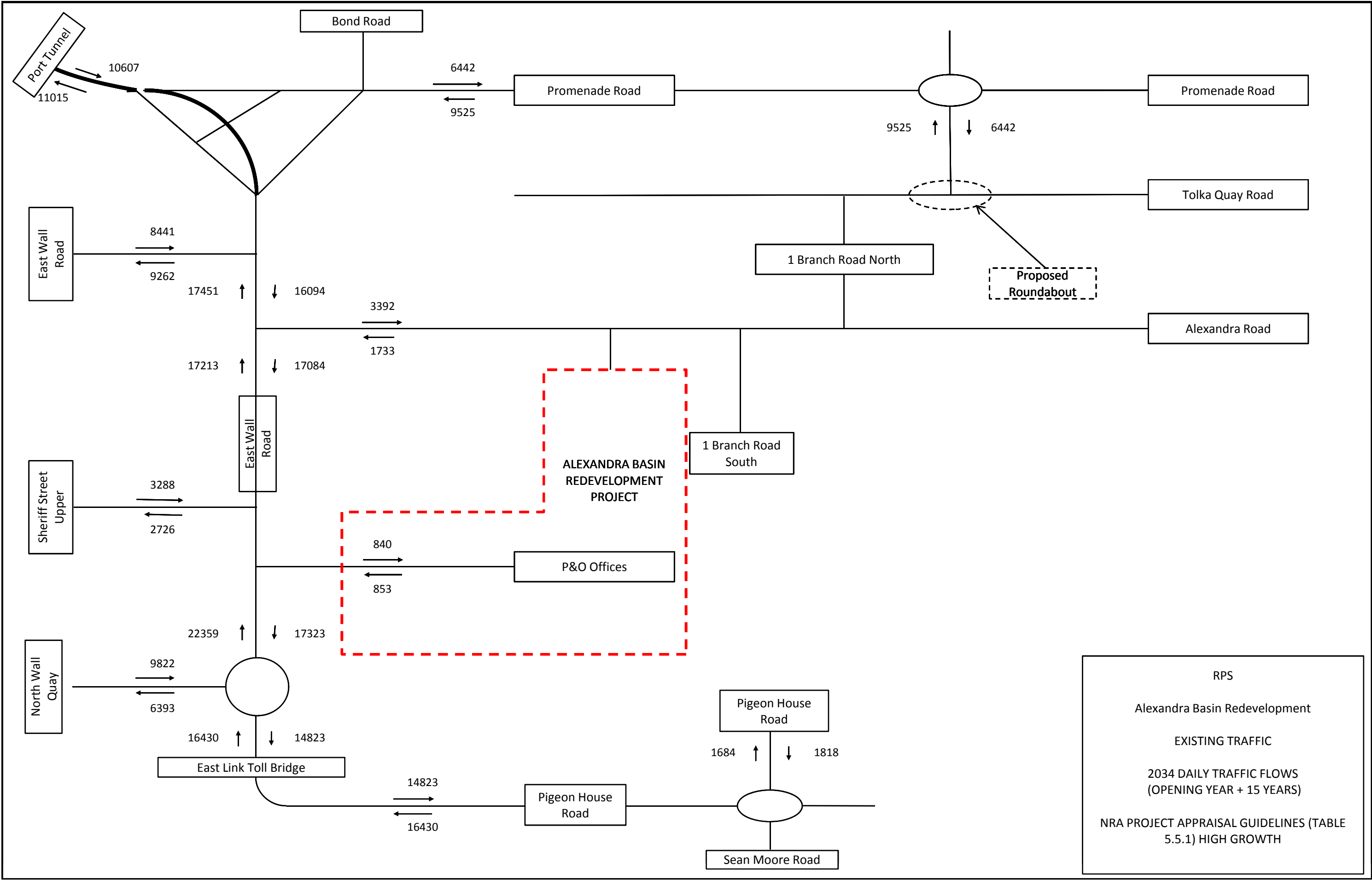


2024 Existing Daily Traffic Flows



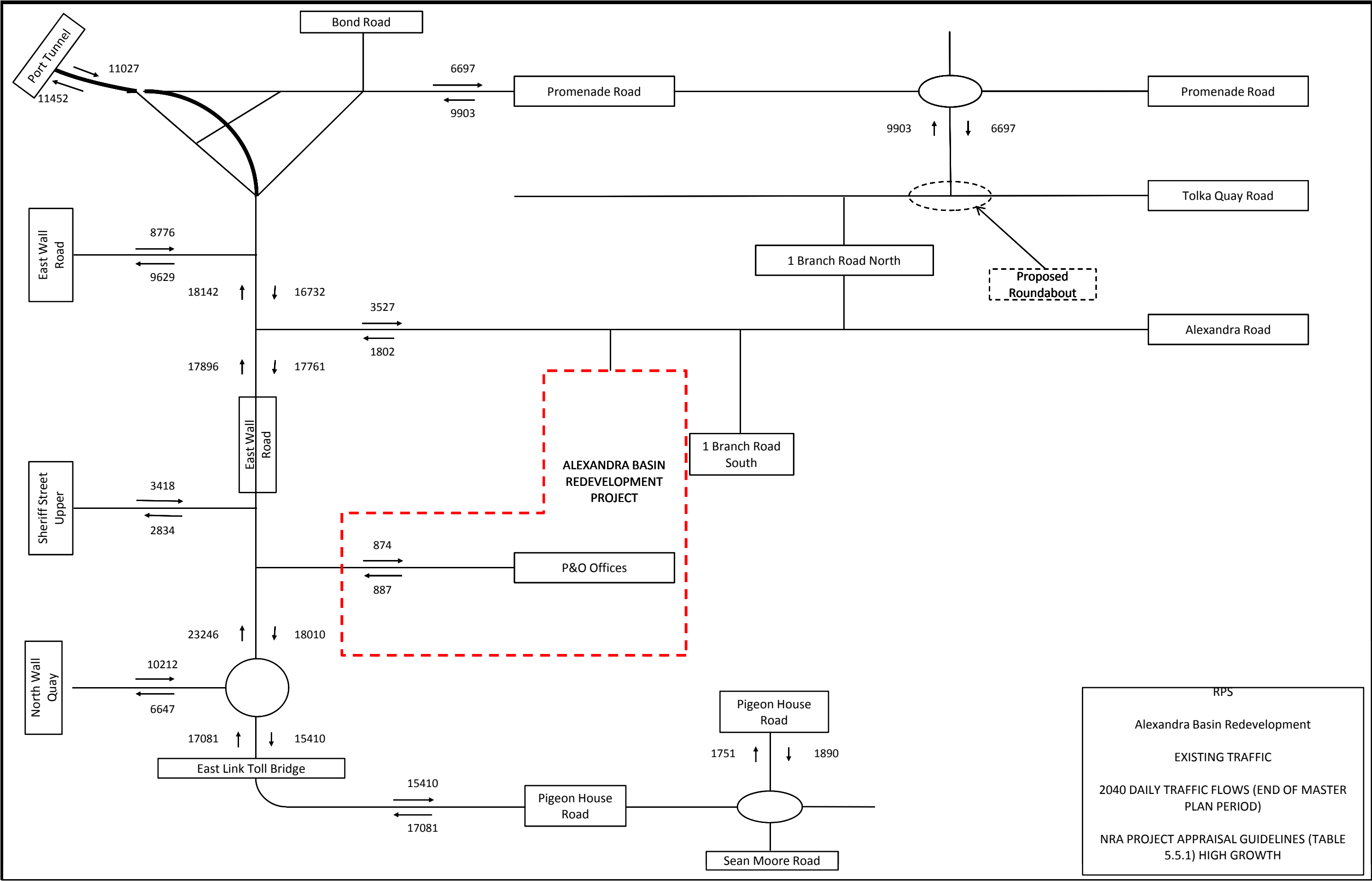


2034 Existing Daily Traffic Flows





2040 Existing Daily Traffic Flows

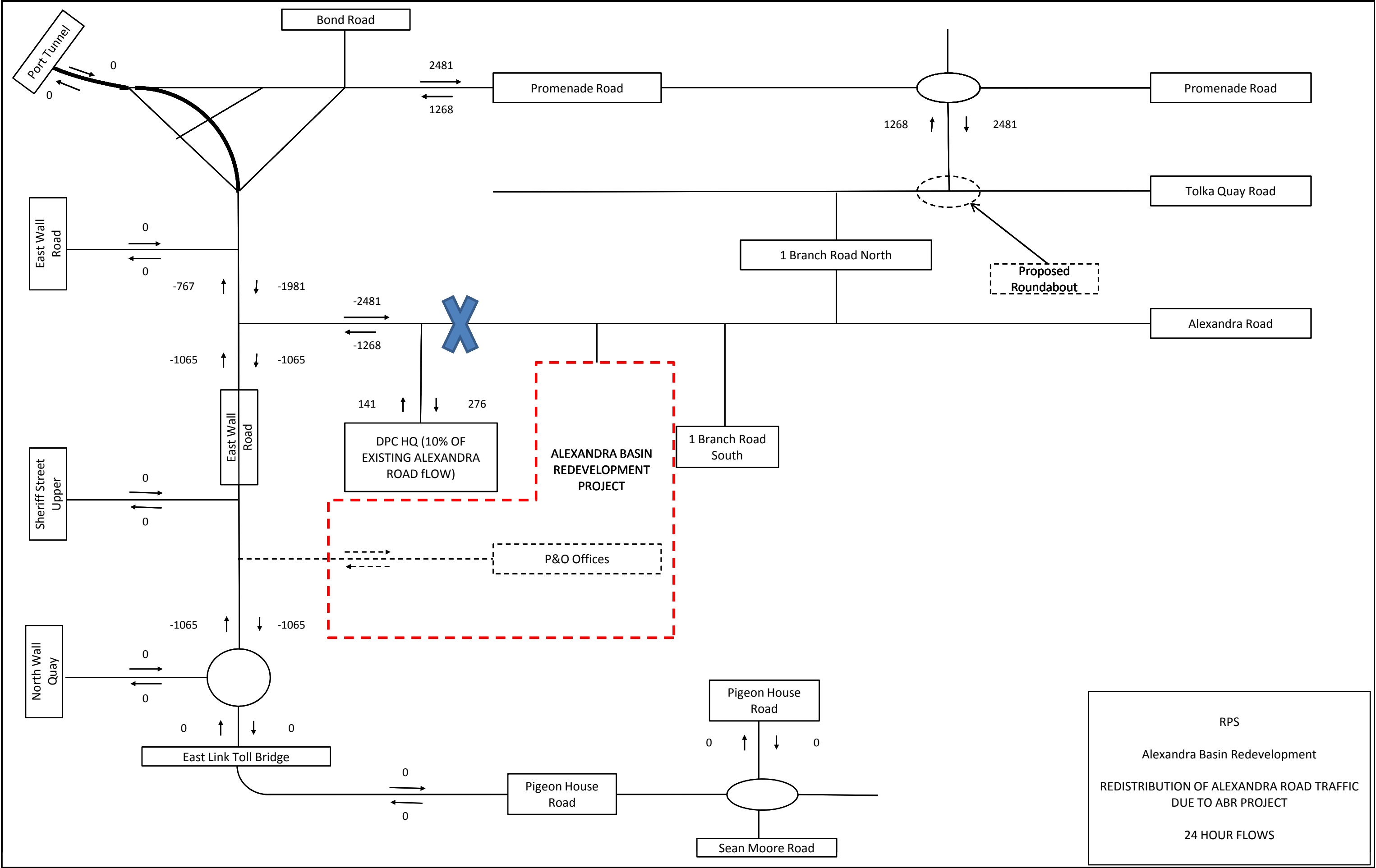




## **Appendix 8.4 Redistribution of Traffic Associated with the Closure of Alexandra Road**

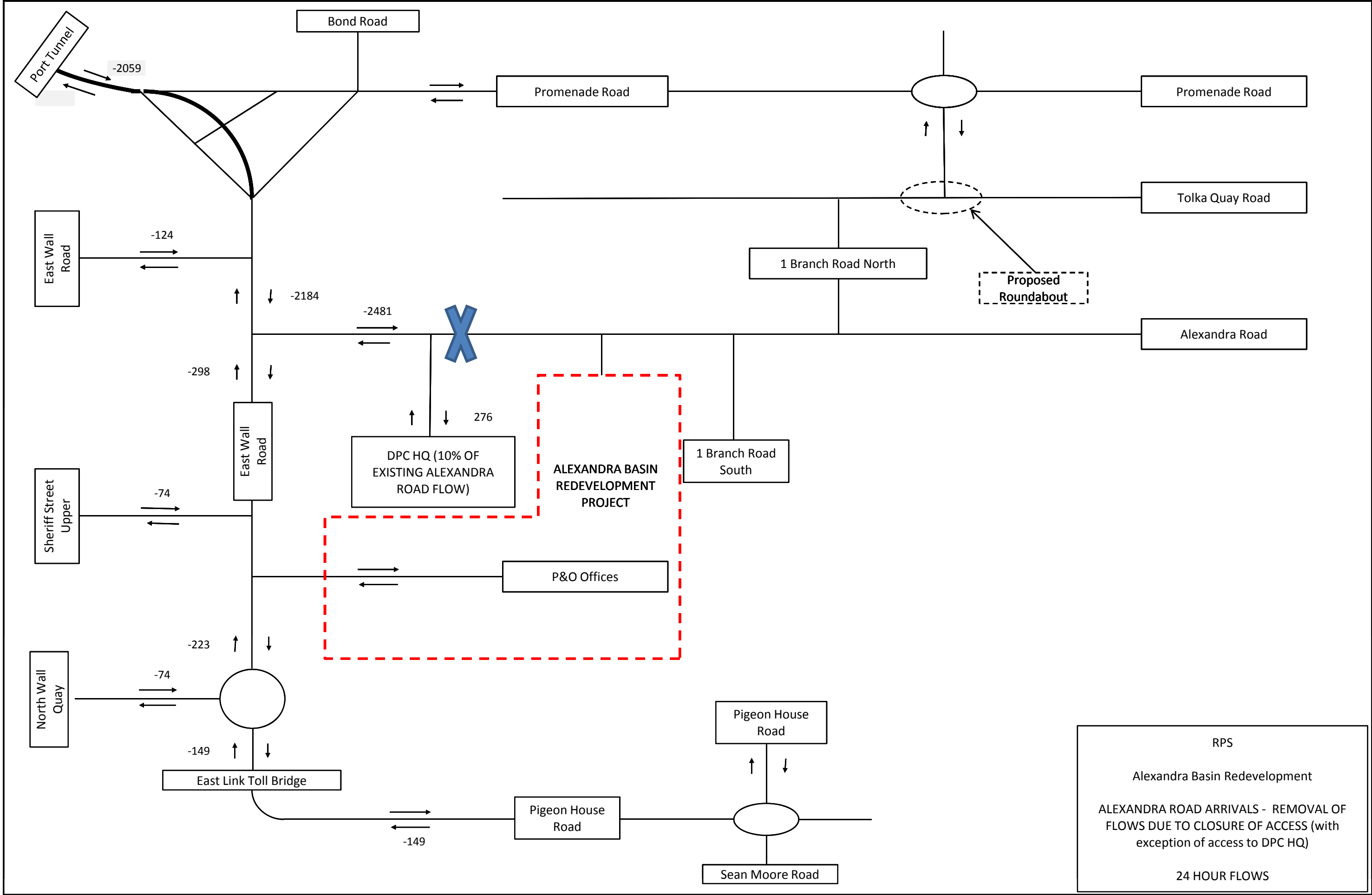


2013 Redistribution of Traffic associated with closure of Alexandra Road due to ABR Project Proposals



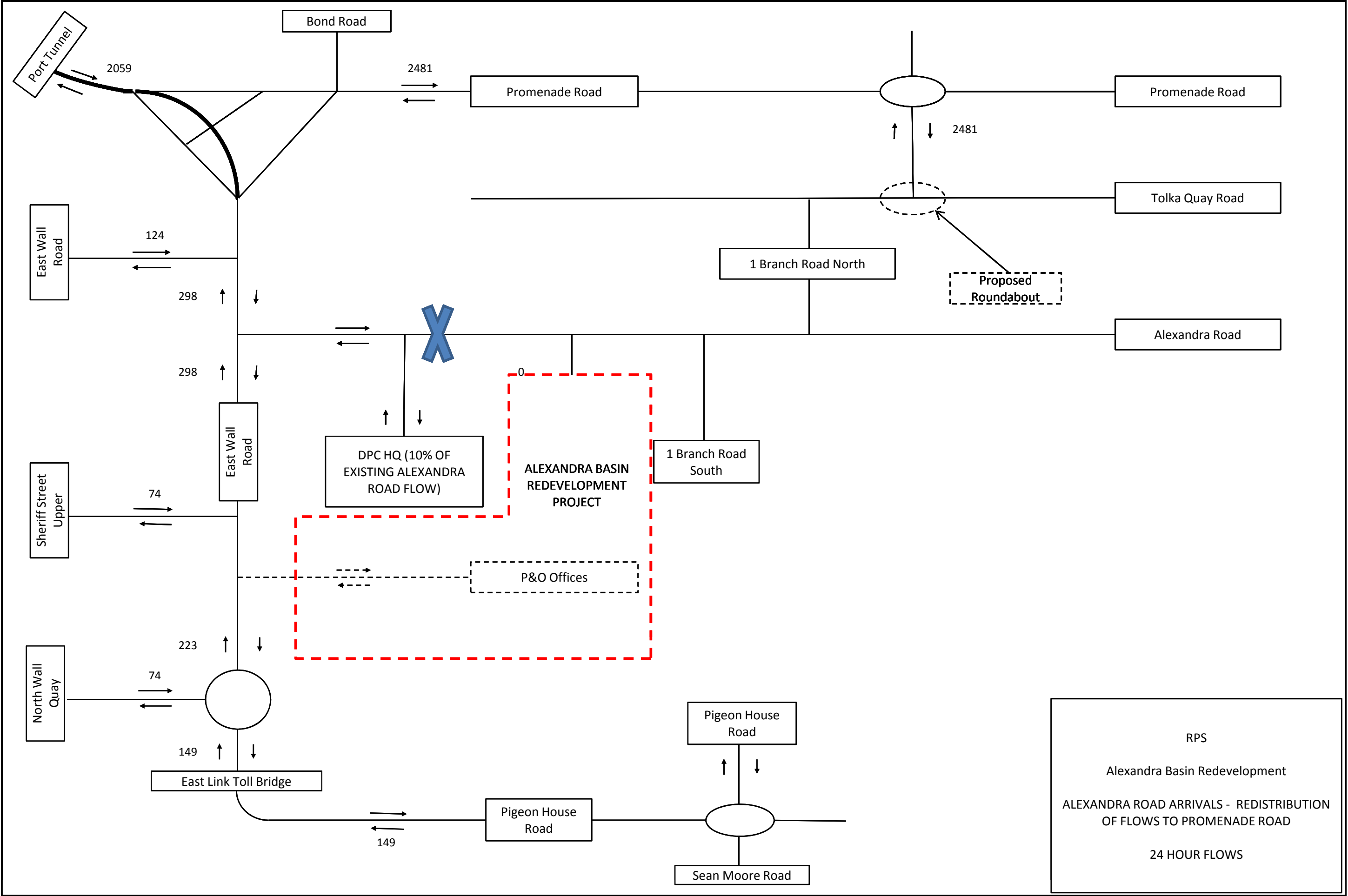


Alexandra Road Arrivals - Removal of Flows due to closure of Access (with exception of DPC HQ)



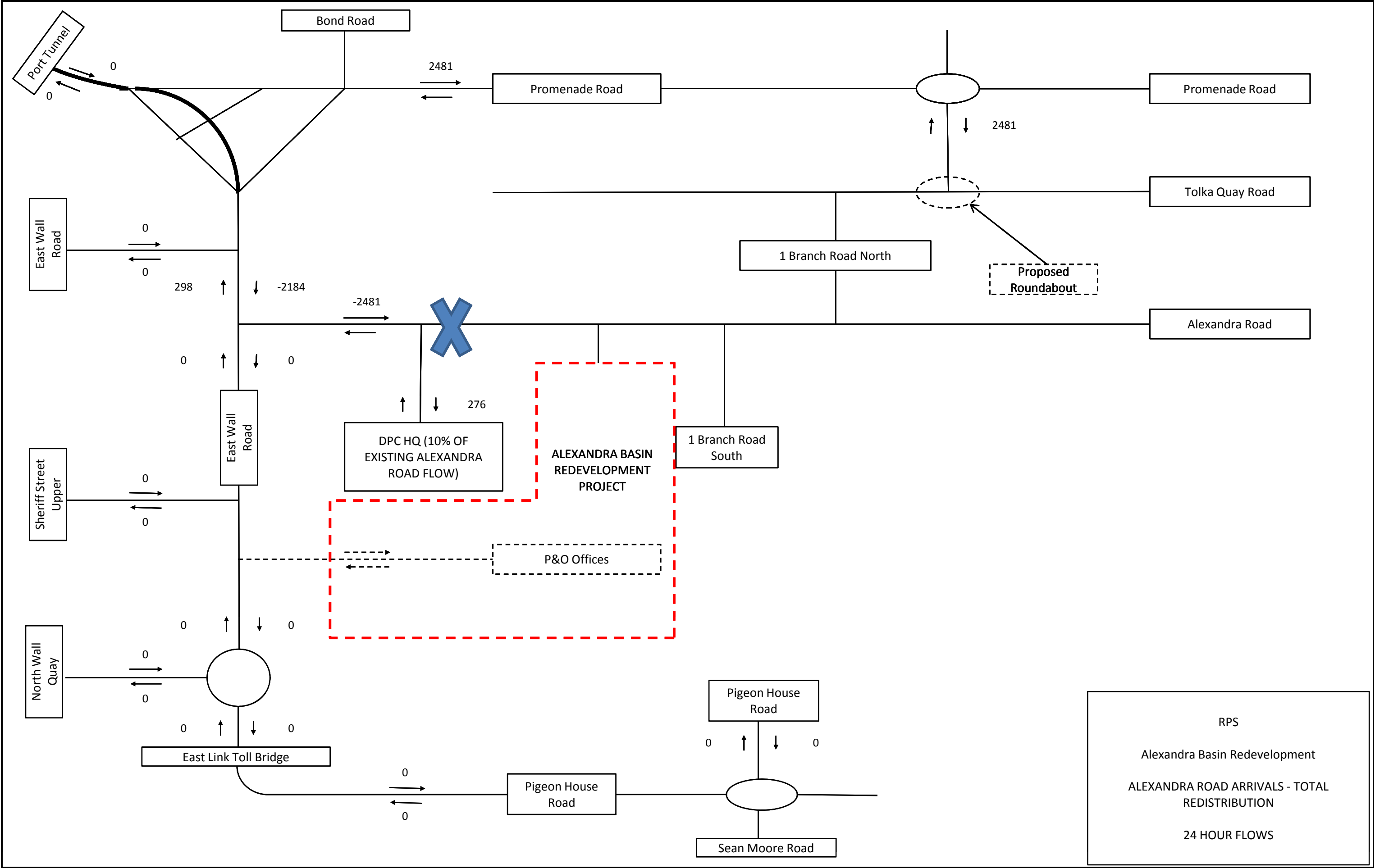


Alexandra Road Arrivals - Redistribution of Flows to Promenade Road



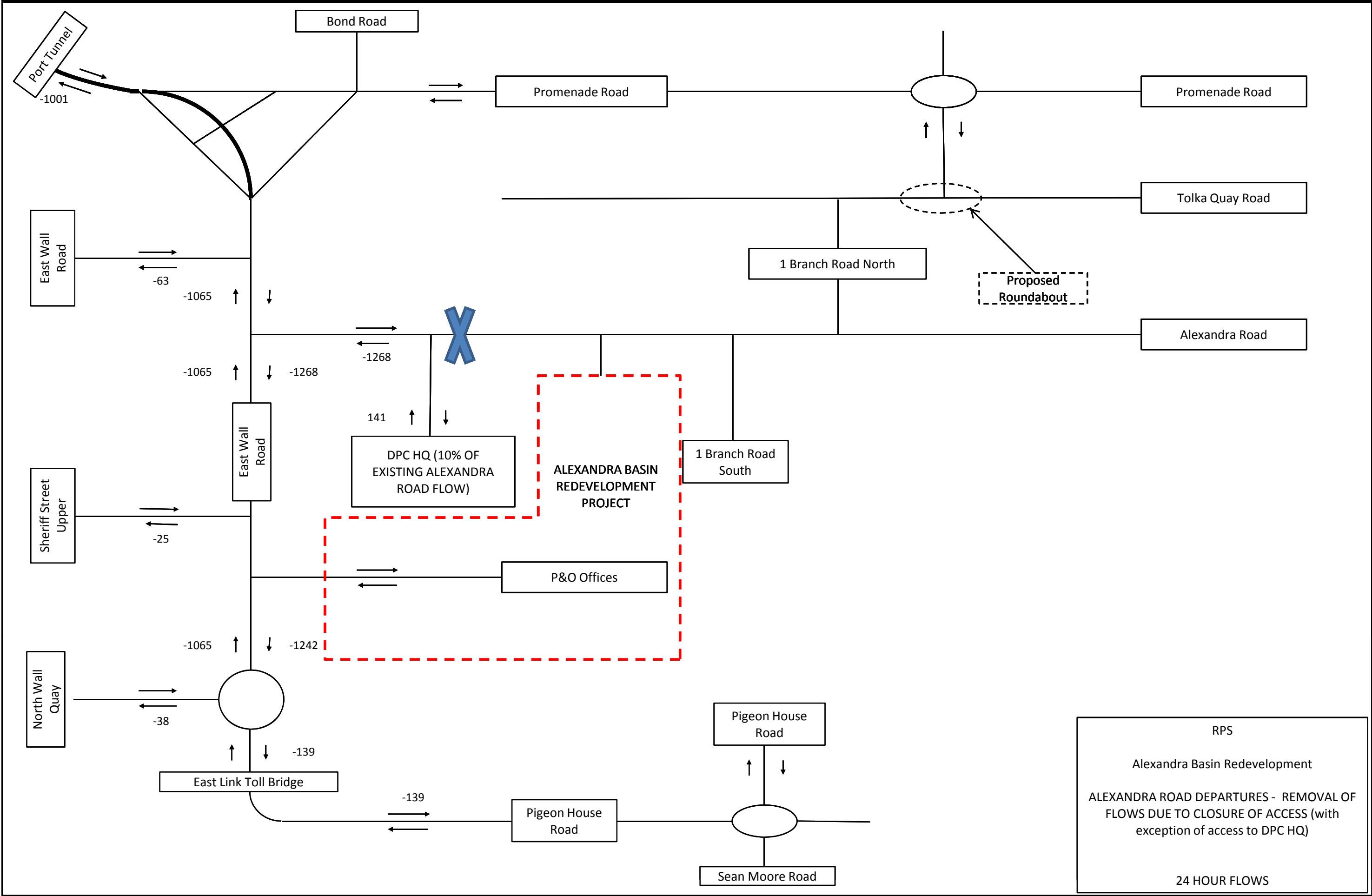


Alexandra Road Arrivals - Total Redistribution of Flows



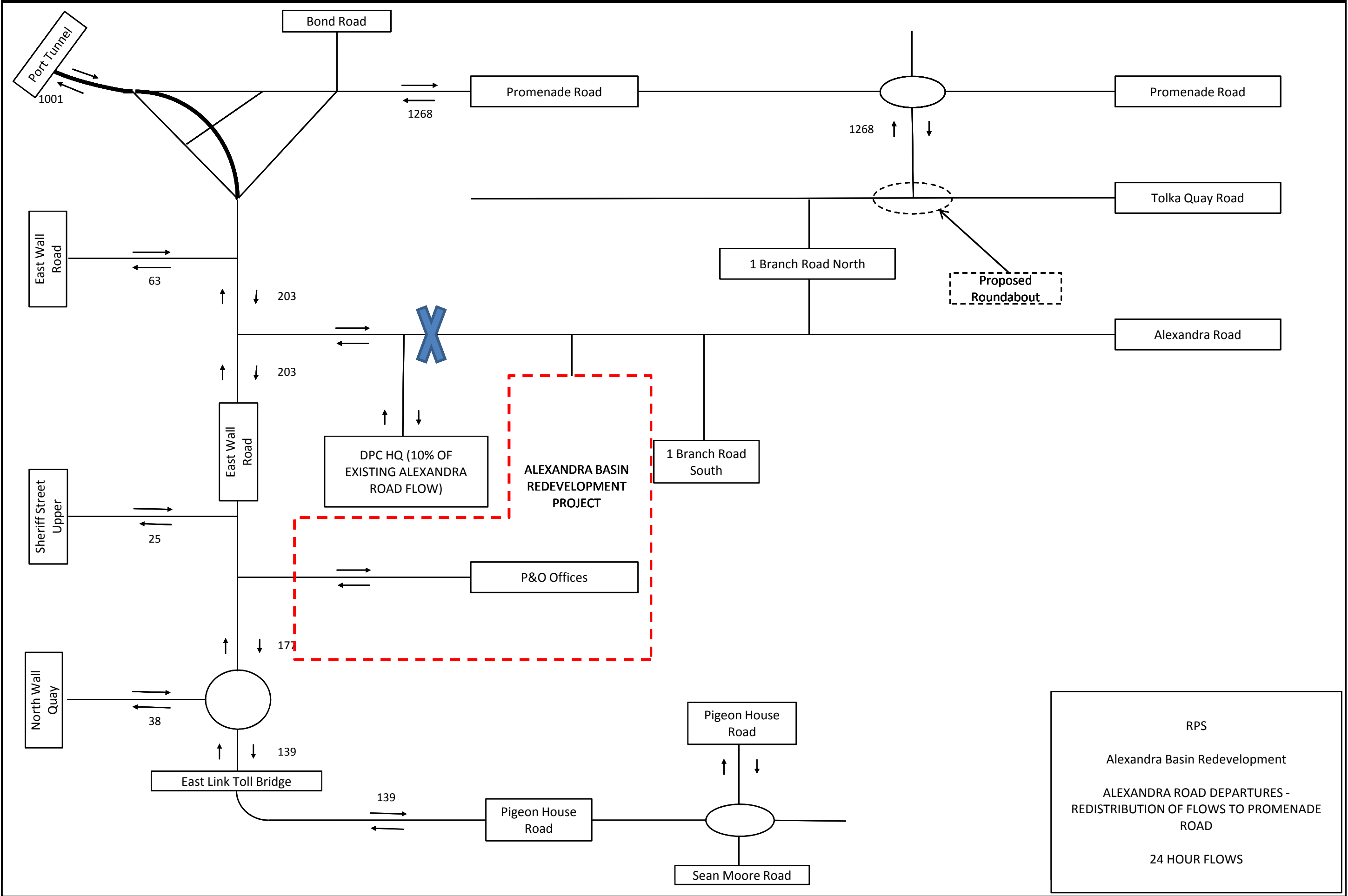


Alexandra Road Departures - Removal of Flows due to closure of Access (with exception of DPC HQ)



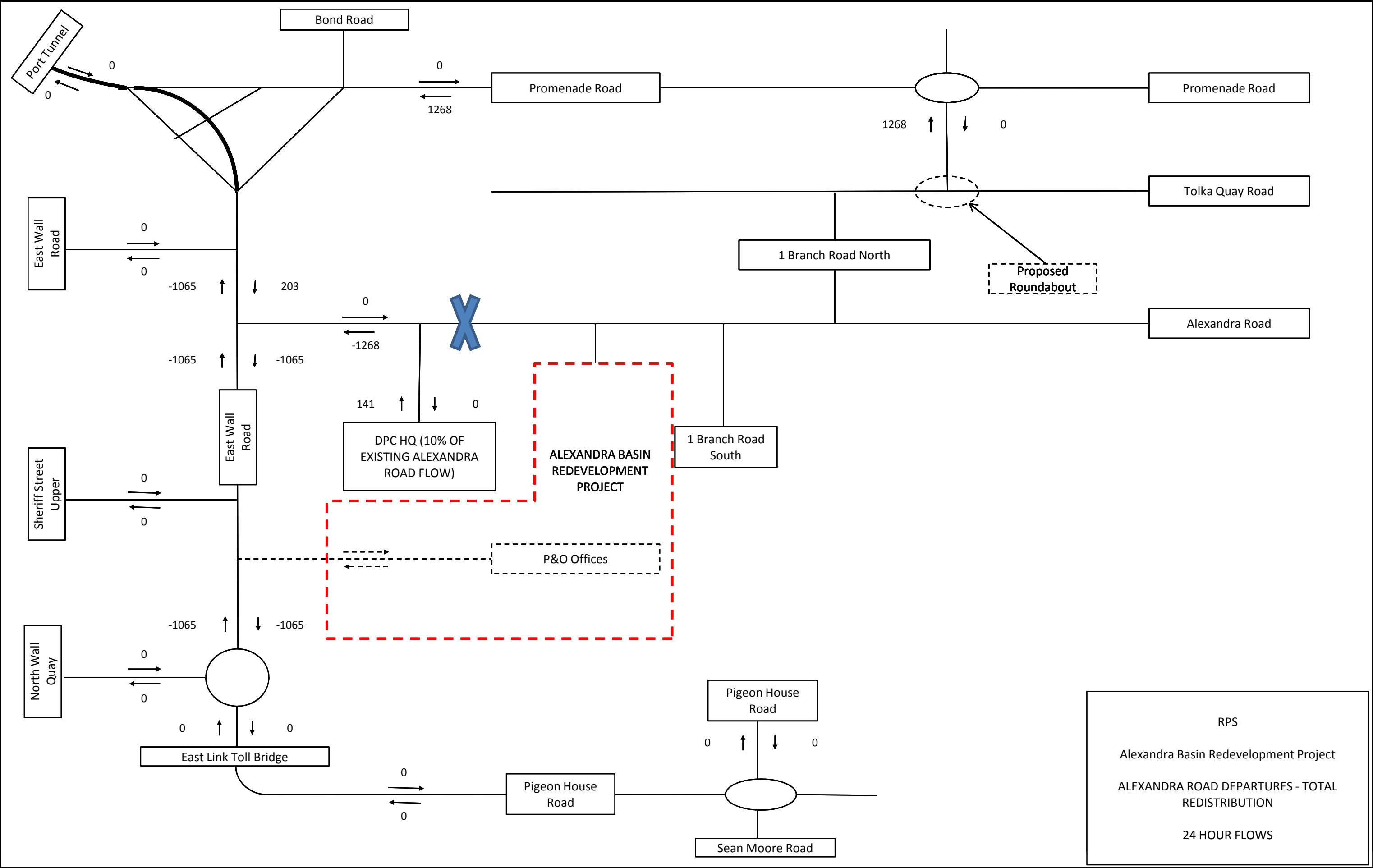


Alexandra Road Departures - Redistribution of Flows to Promenade Road





Alexandra Road Departures - Total Redistribution of Flows

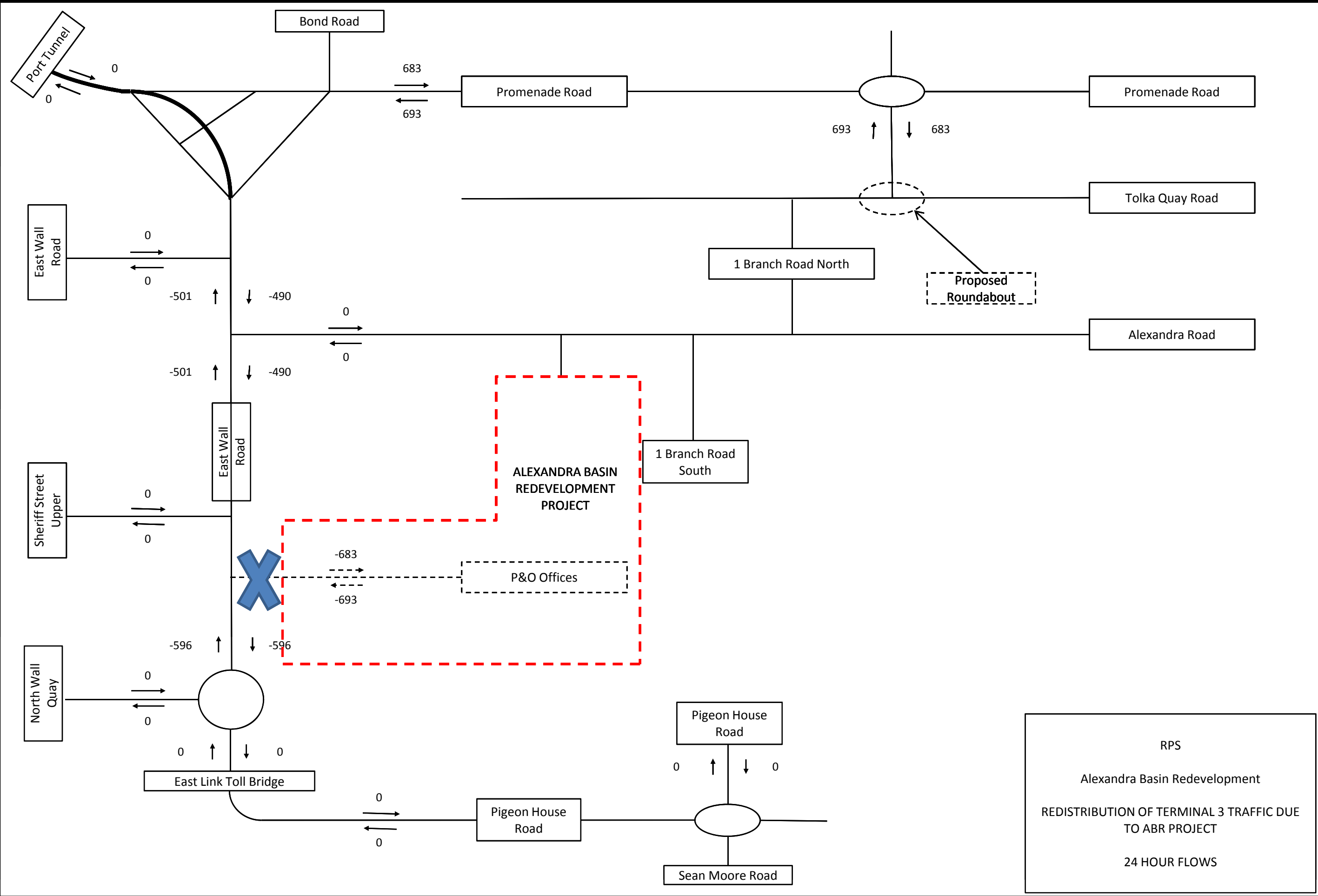




## **Appendix 8.5 Redistribution of Traffic Associated with the Closure of Terminal 3 Access**

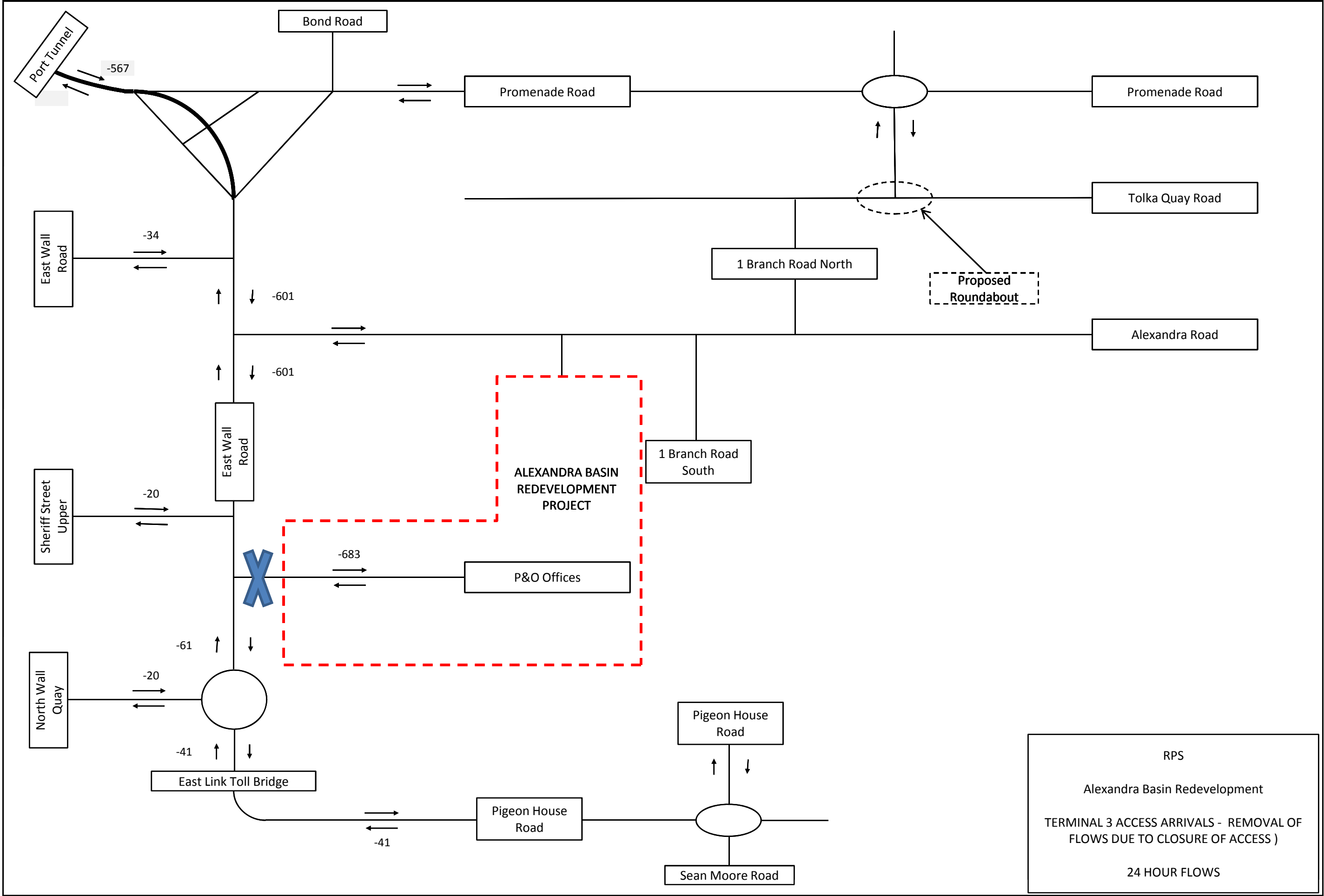


2013 Redistribution of Traffic associated with closure of Terminal 3 due to ABR Project Proposals



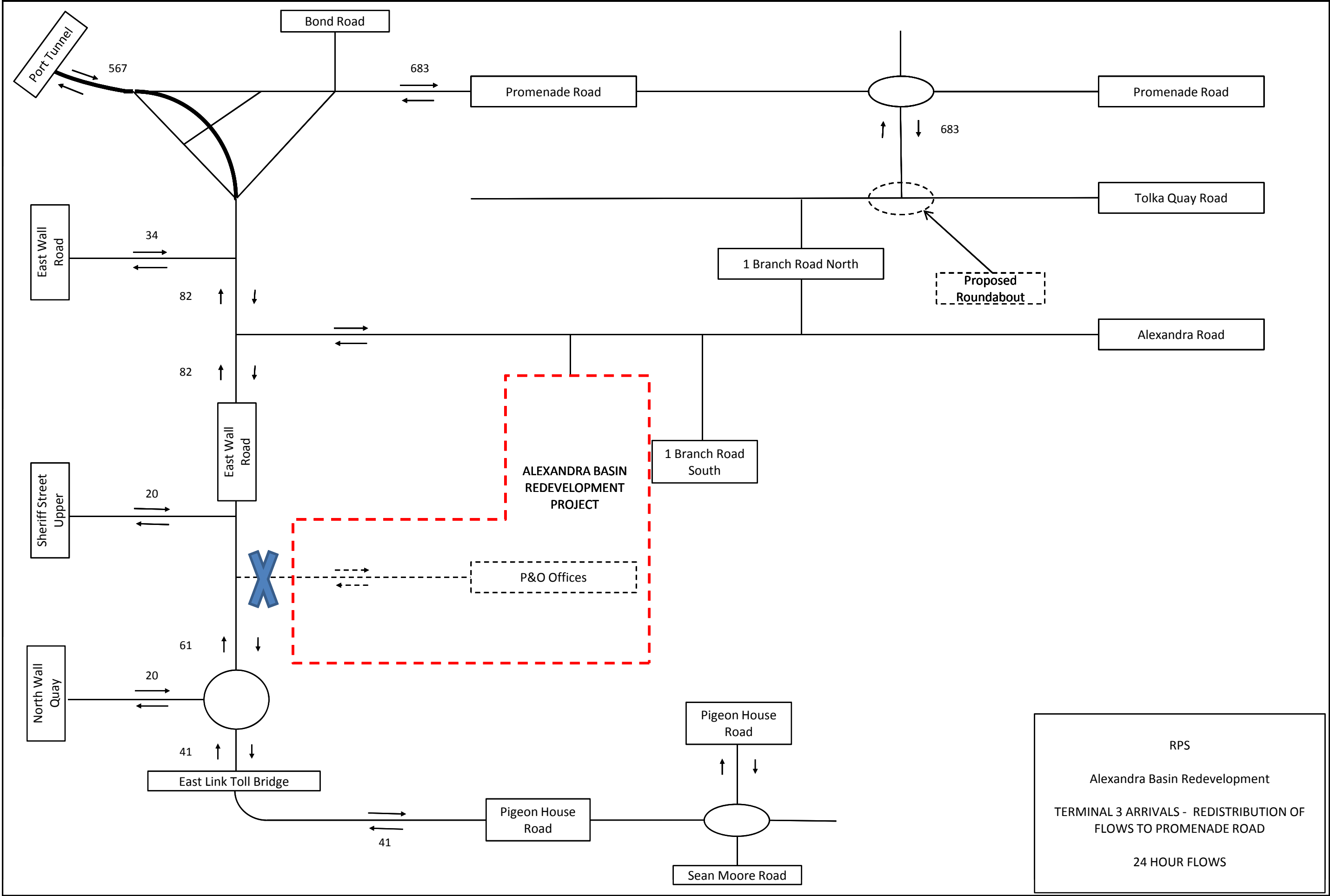


Terminal 3 Arrivals - Removal of Flows Due to Closure of Access



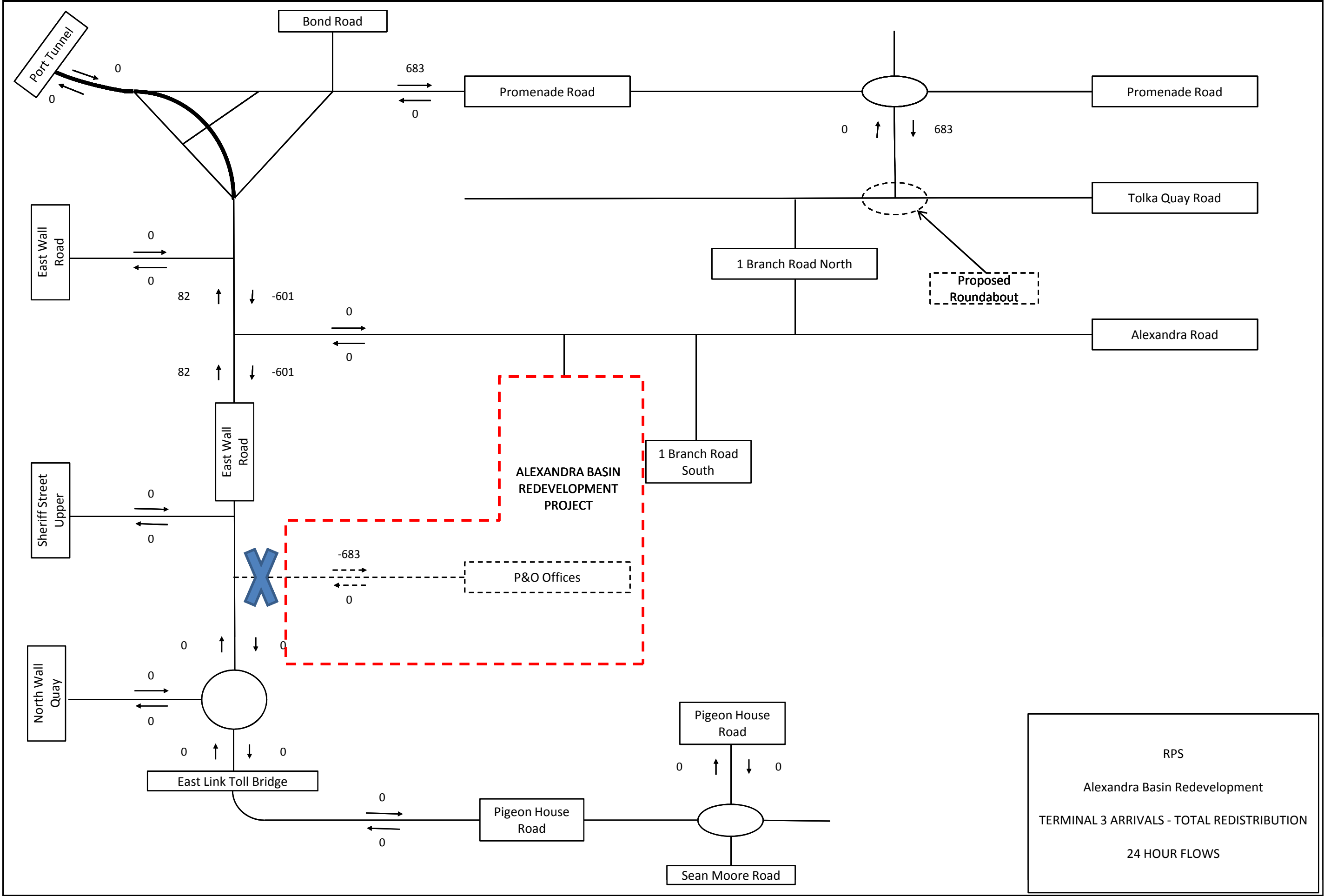


Terminal 3 Arrivals - Redistribution of Flows to Promenade Road





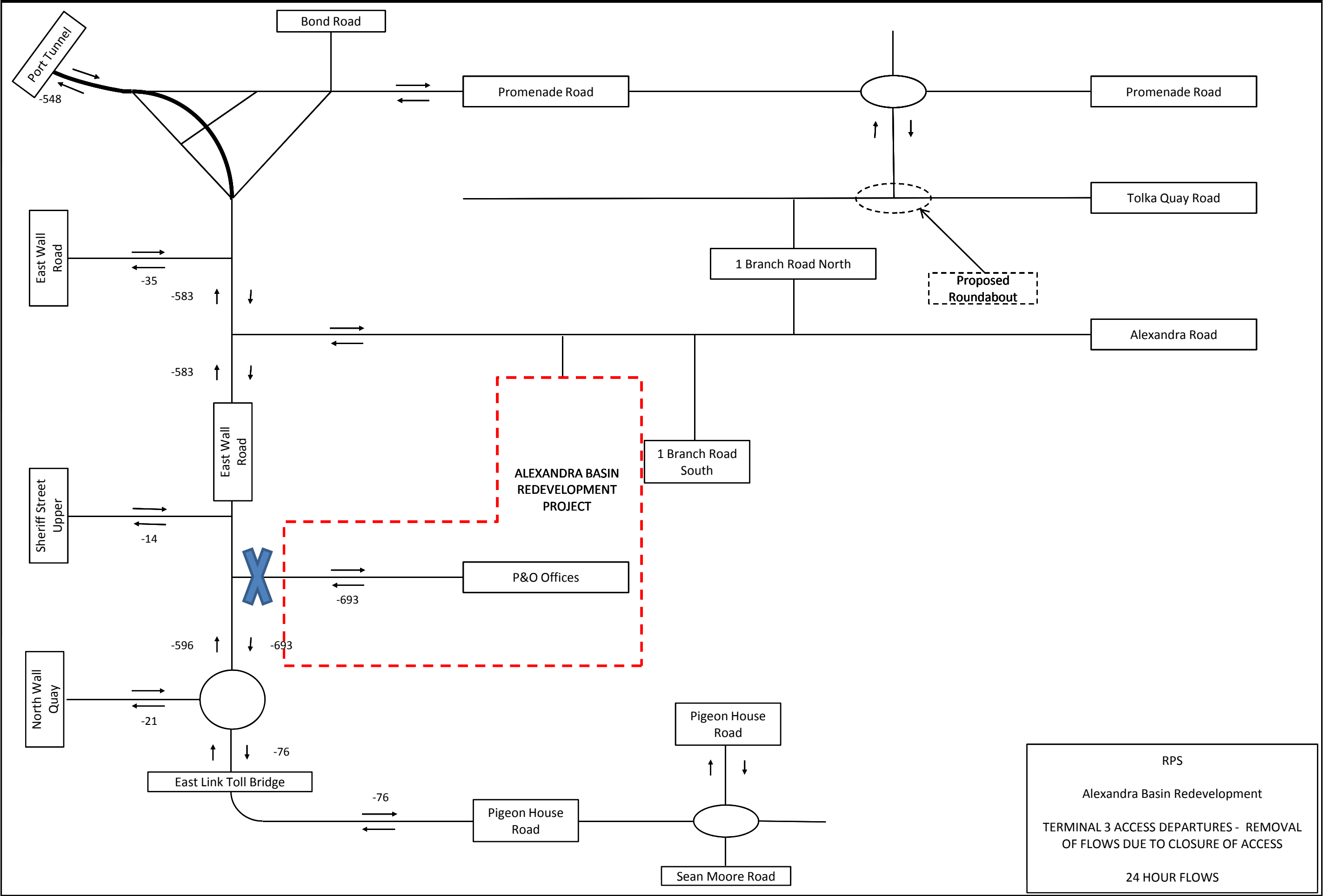
Terminal 3 Arrivals - Total Redistribution of Flows



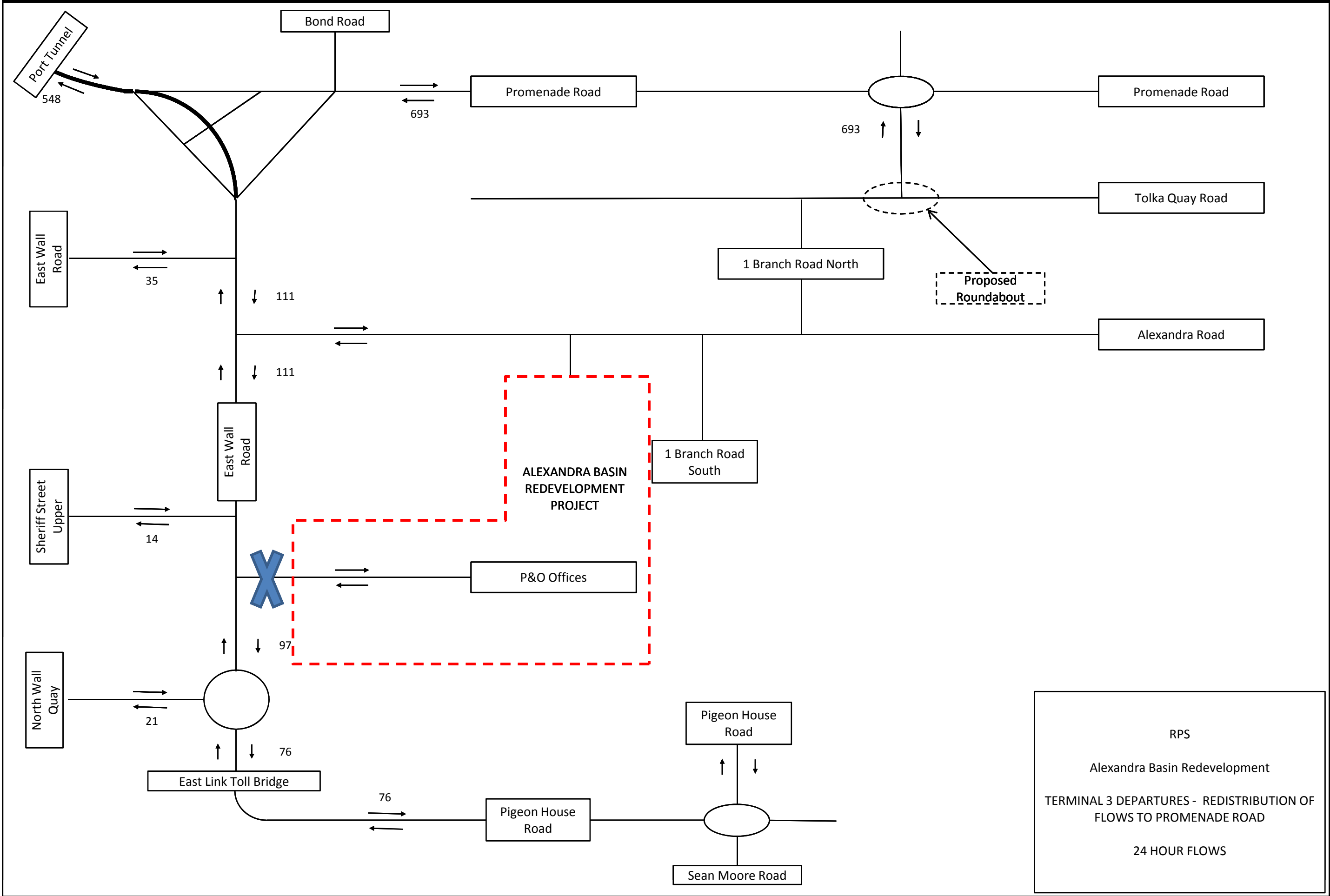
RPS  
Alexandra Basin Redevelopment  
TERMINAL 3 ARRIVALS - TOTAL REDISTRIBUTION  
24 HOUR FLOWS



Terminal 3 Departures - Removal of Flows Due to Closure of Access

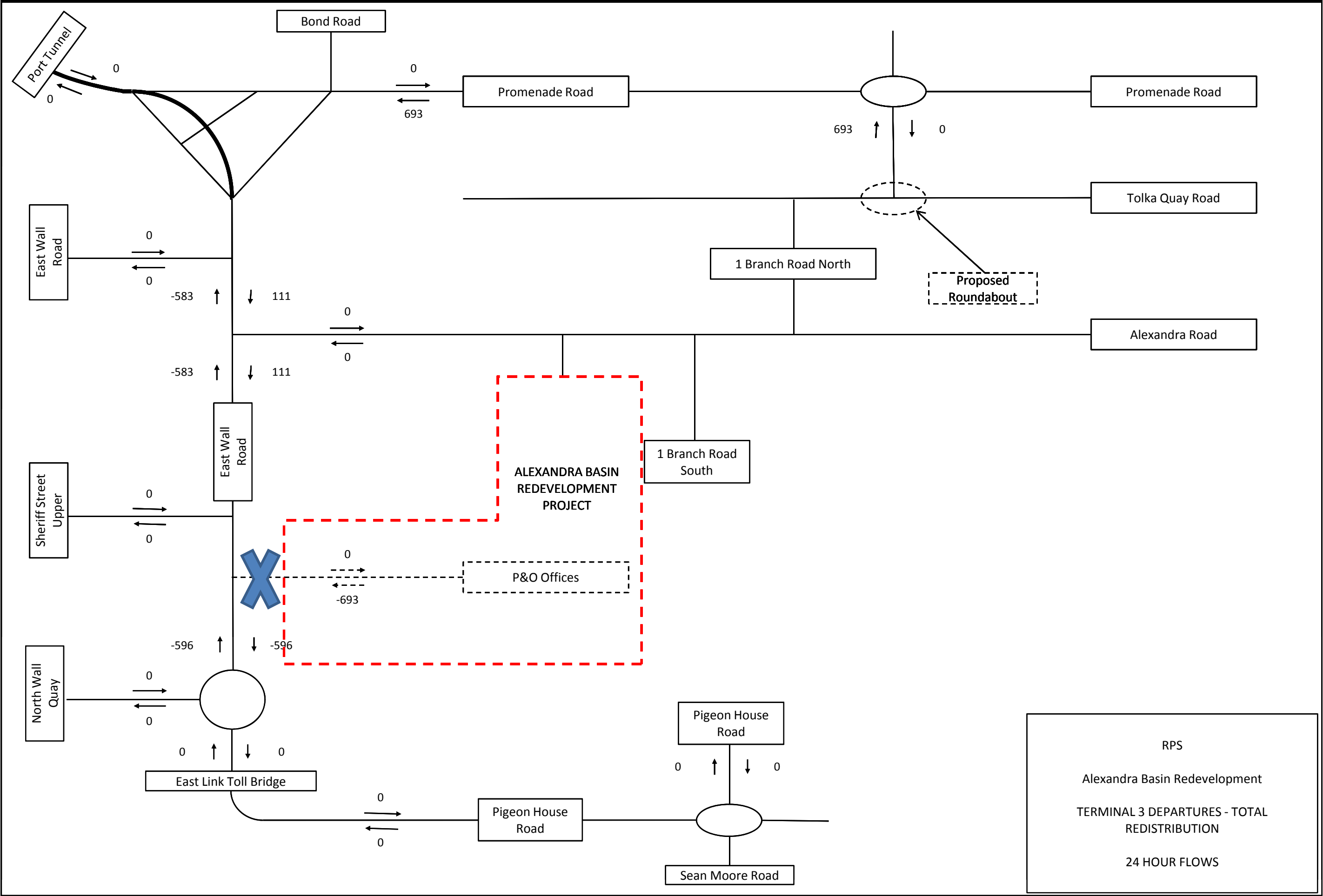


Terminal 3 Departures - Redistribution of Flows to Promenade Road





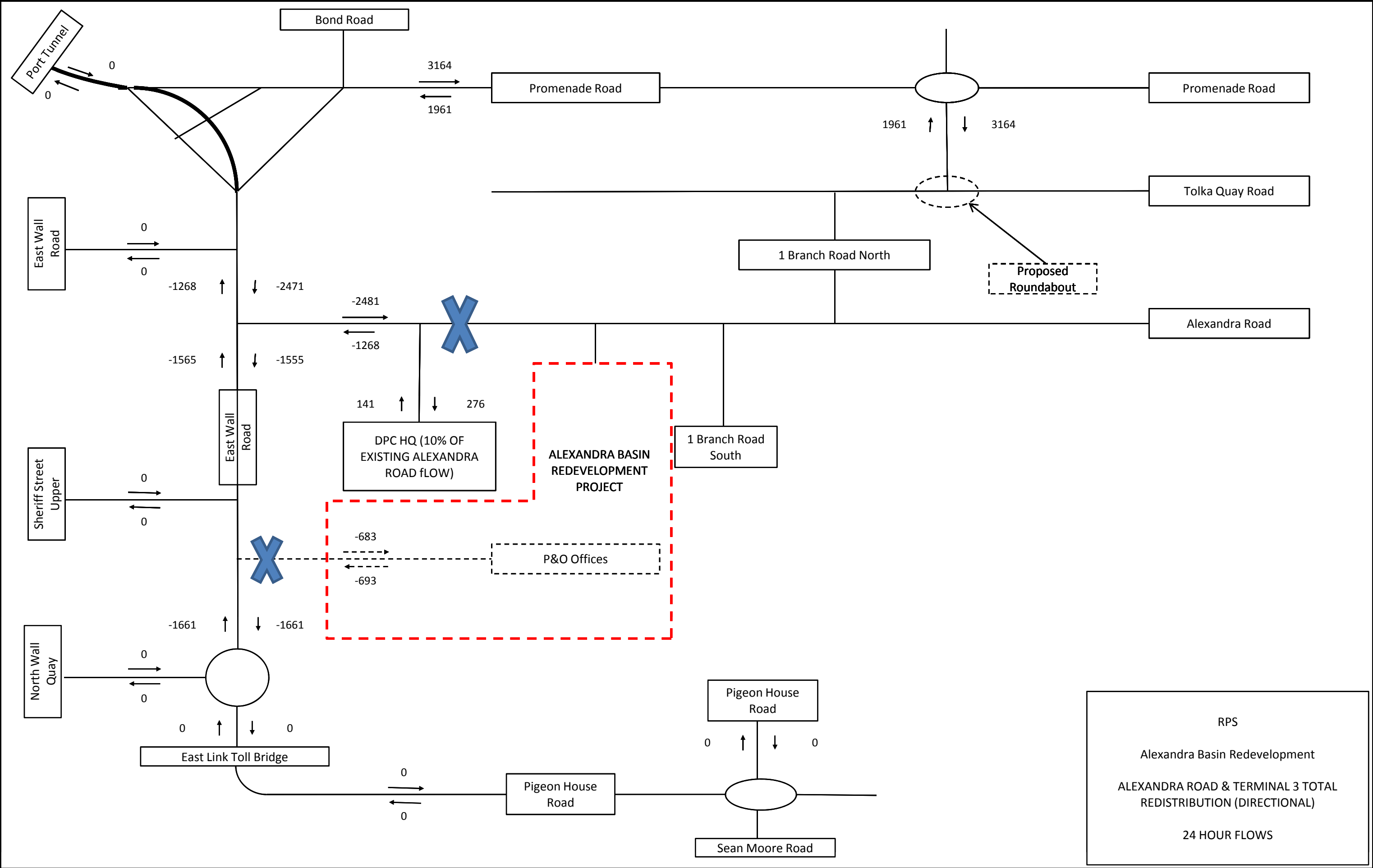
Terminal 3 Departures - Total Redistribution of Flows



**Appendix 8.6 Total Redistribution of Existing Traffic Due to the ABR Project**



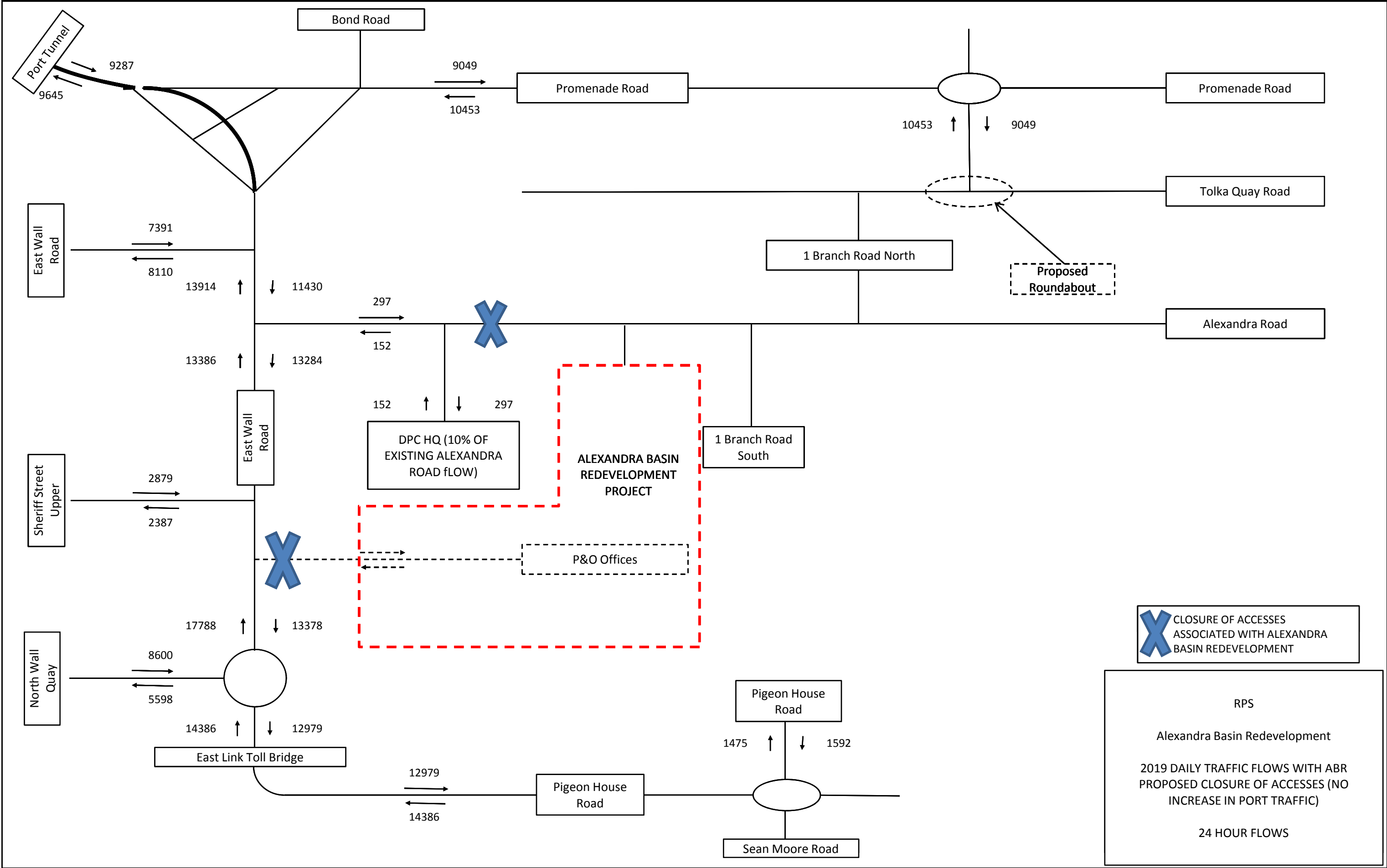
2013 Redistribution of Traffic associated with closure of Alexandra Road and Terminal 3 Access due to ABR Project Proposals



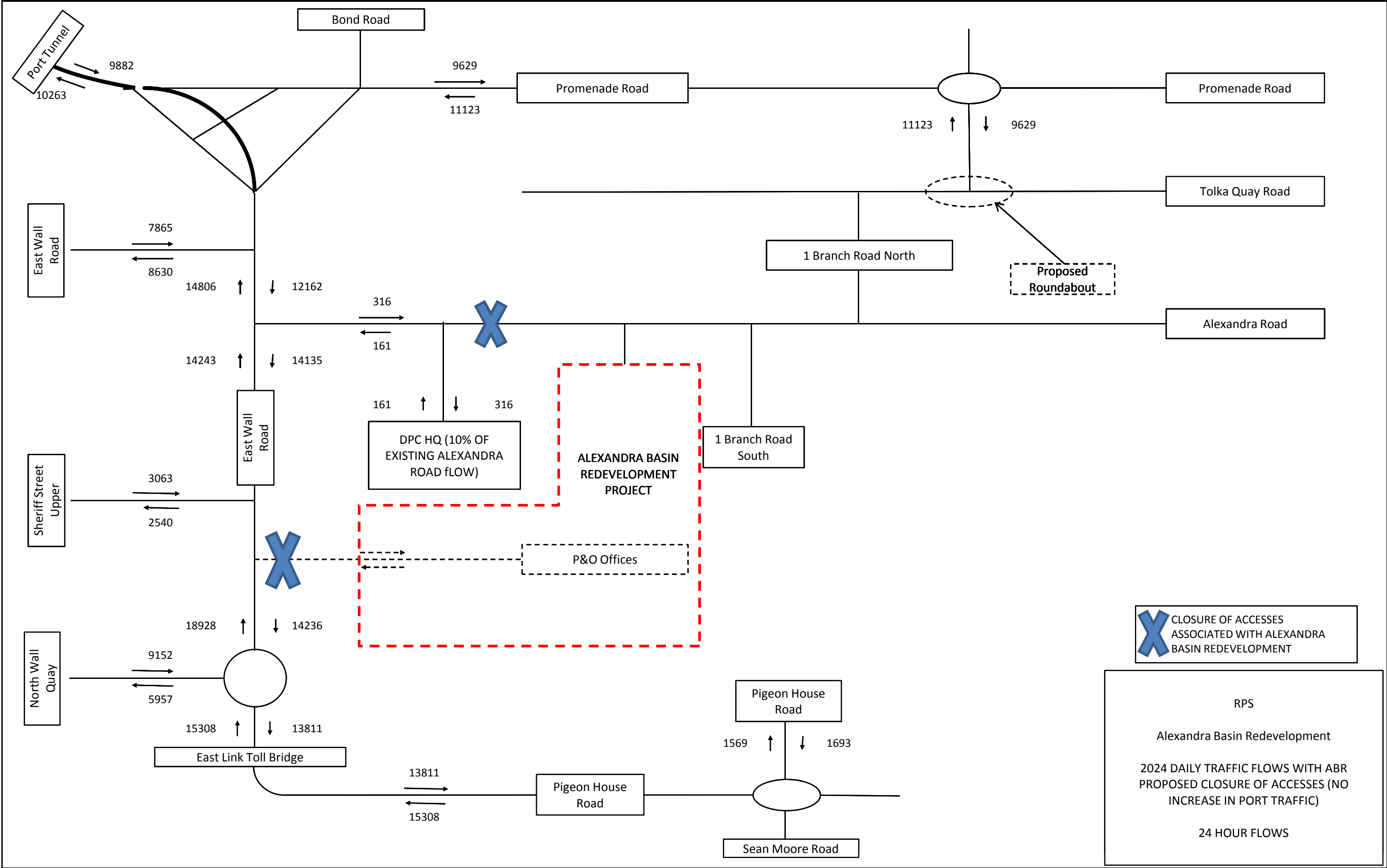
**Appendix 8.7 Impact of Existing Port Traffic Flows in Future Years**



2019 Existing Traffic Flows including Redistribution associated with ABR Project

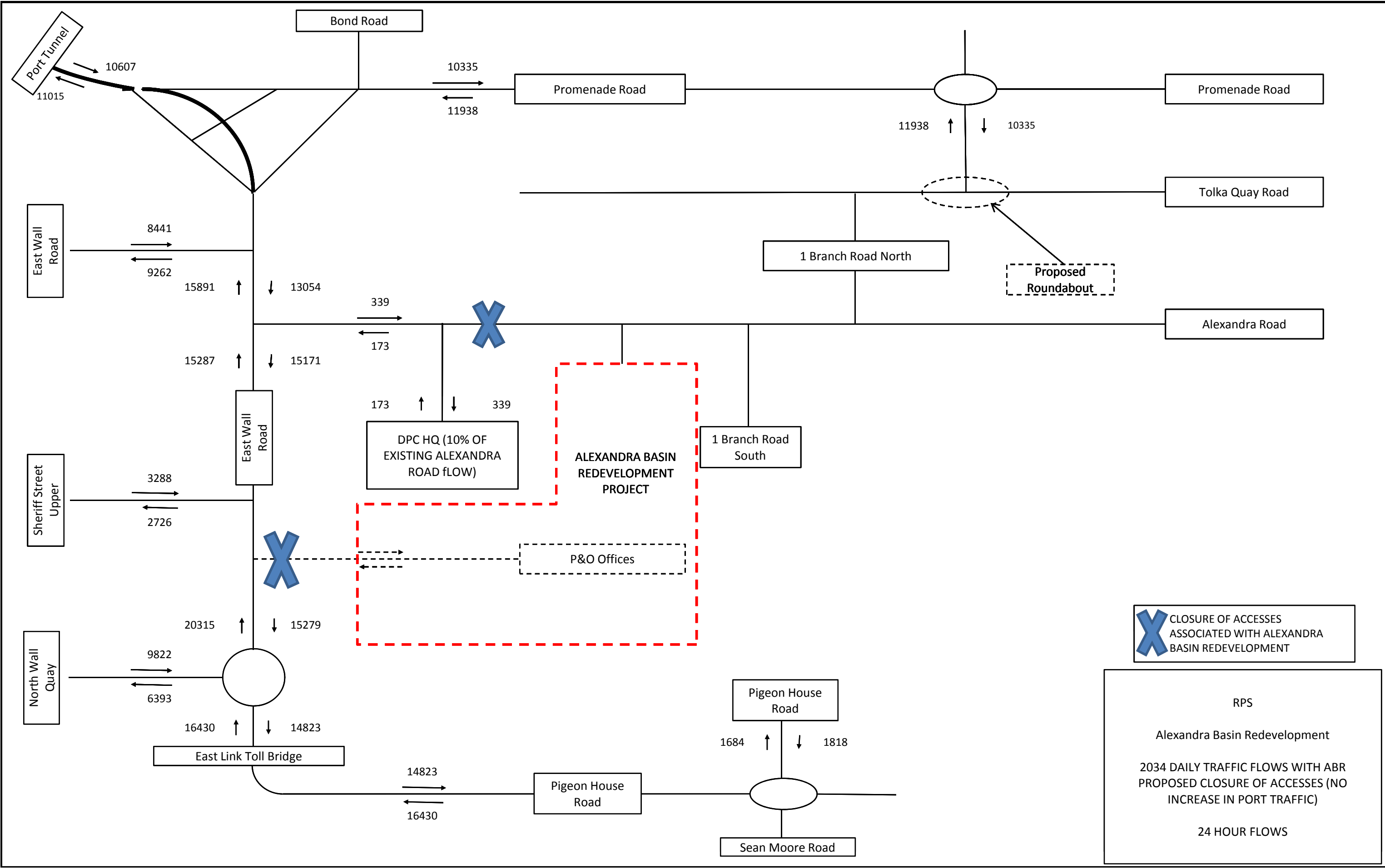


2024 Existing Traffic Flows including Redistribution associated with ABR Project

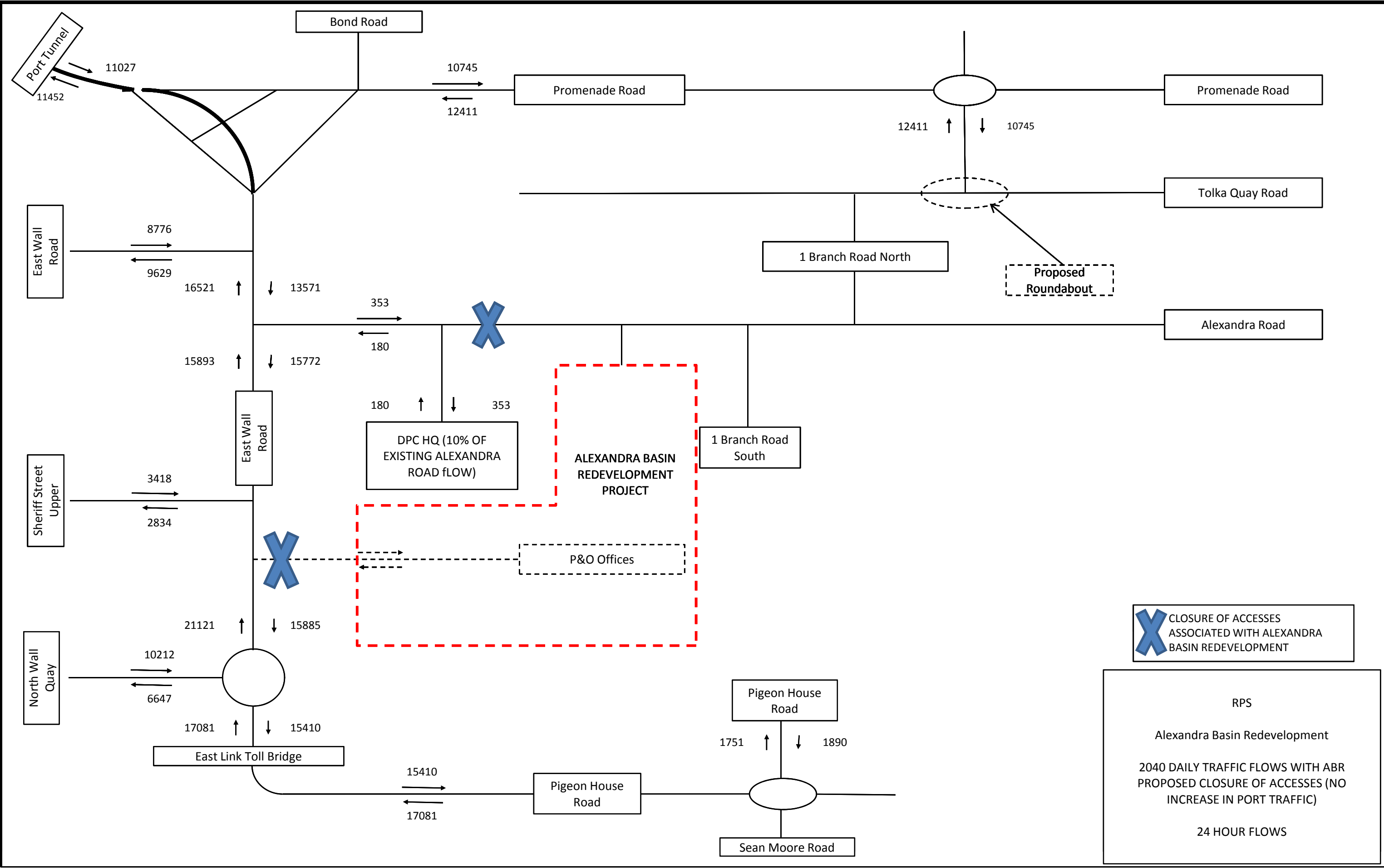




2034 Existing Traffic Flows including Redistribution associated with ABR Project



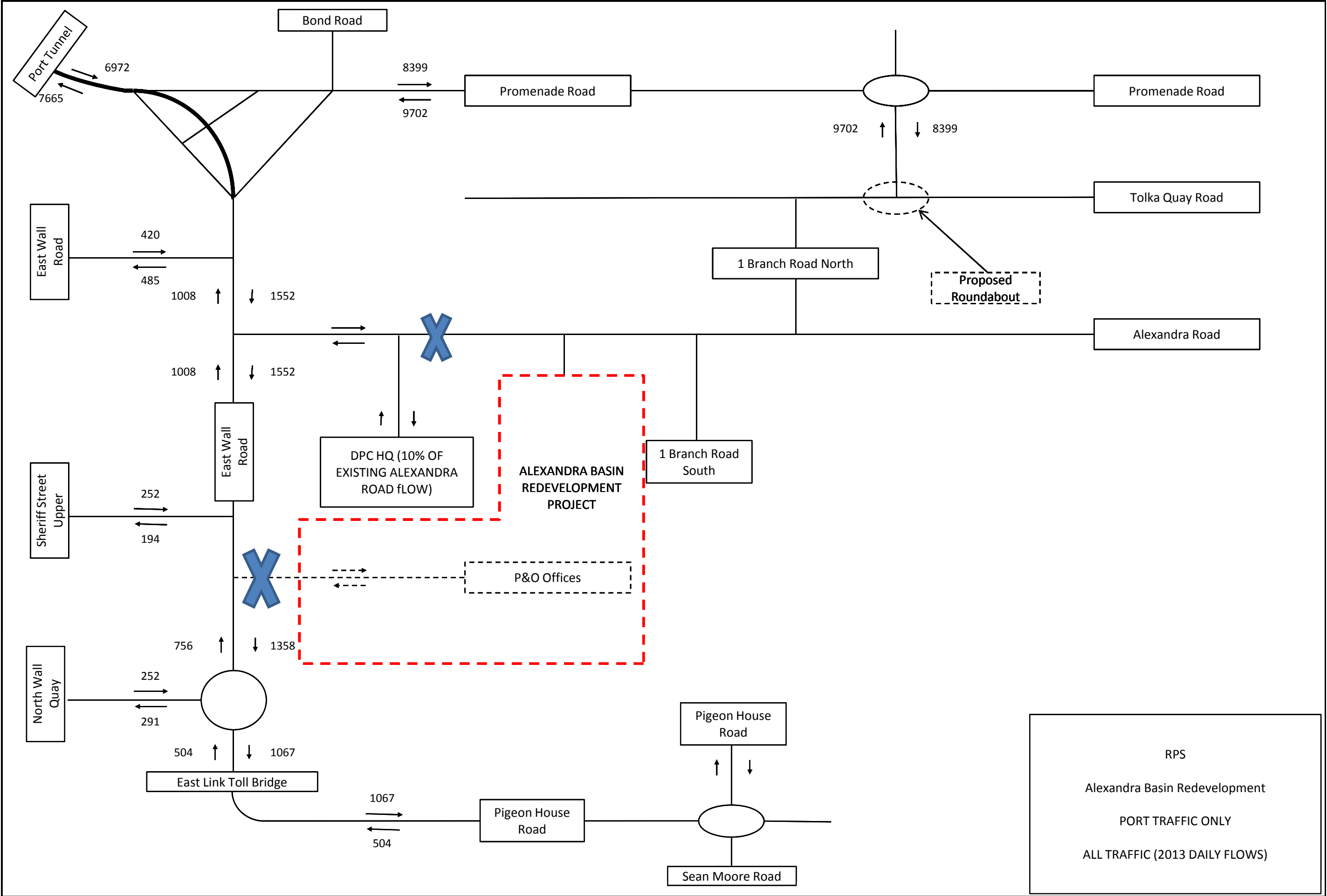
2040 Existing Traffic Flows including Redistribution associated with ABR Project





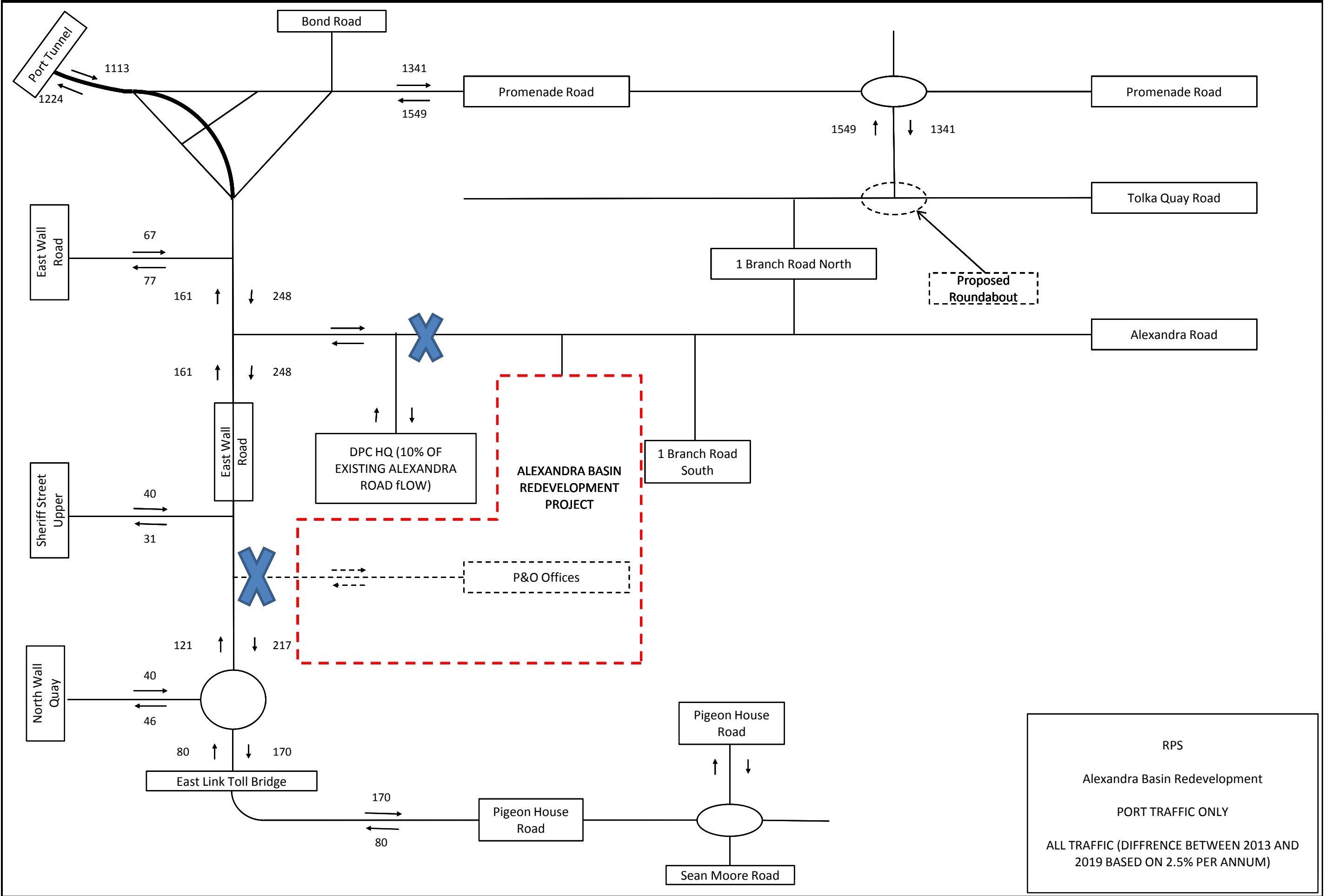
**Appendix 8.8 Growth in Port Traffic Flows in Future Years**

2013 Existing Port Traffic Only

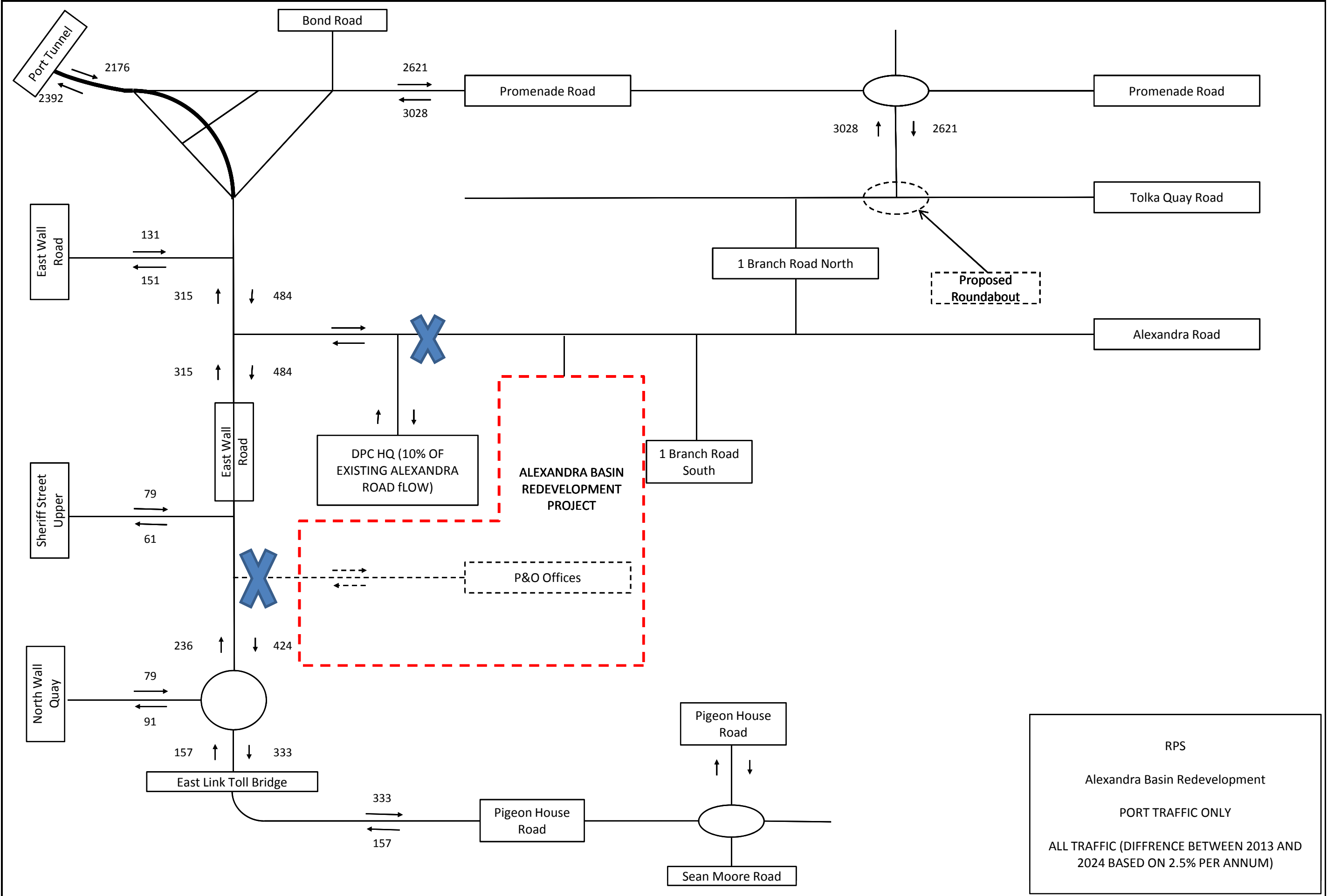




Increase in Port Traffic 2013 - 2019

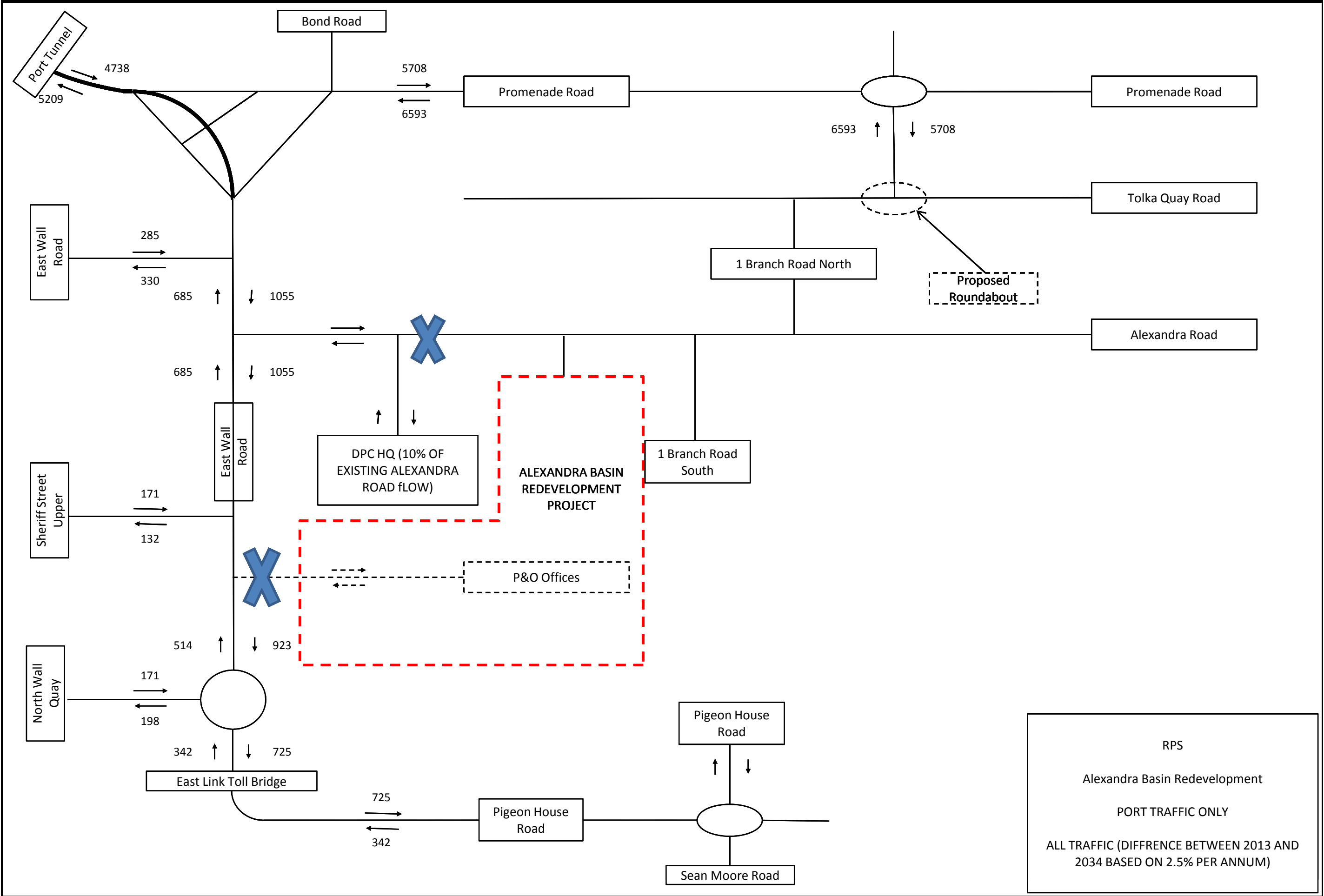


Increase in Port Traffic 2013 - 2024

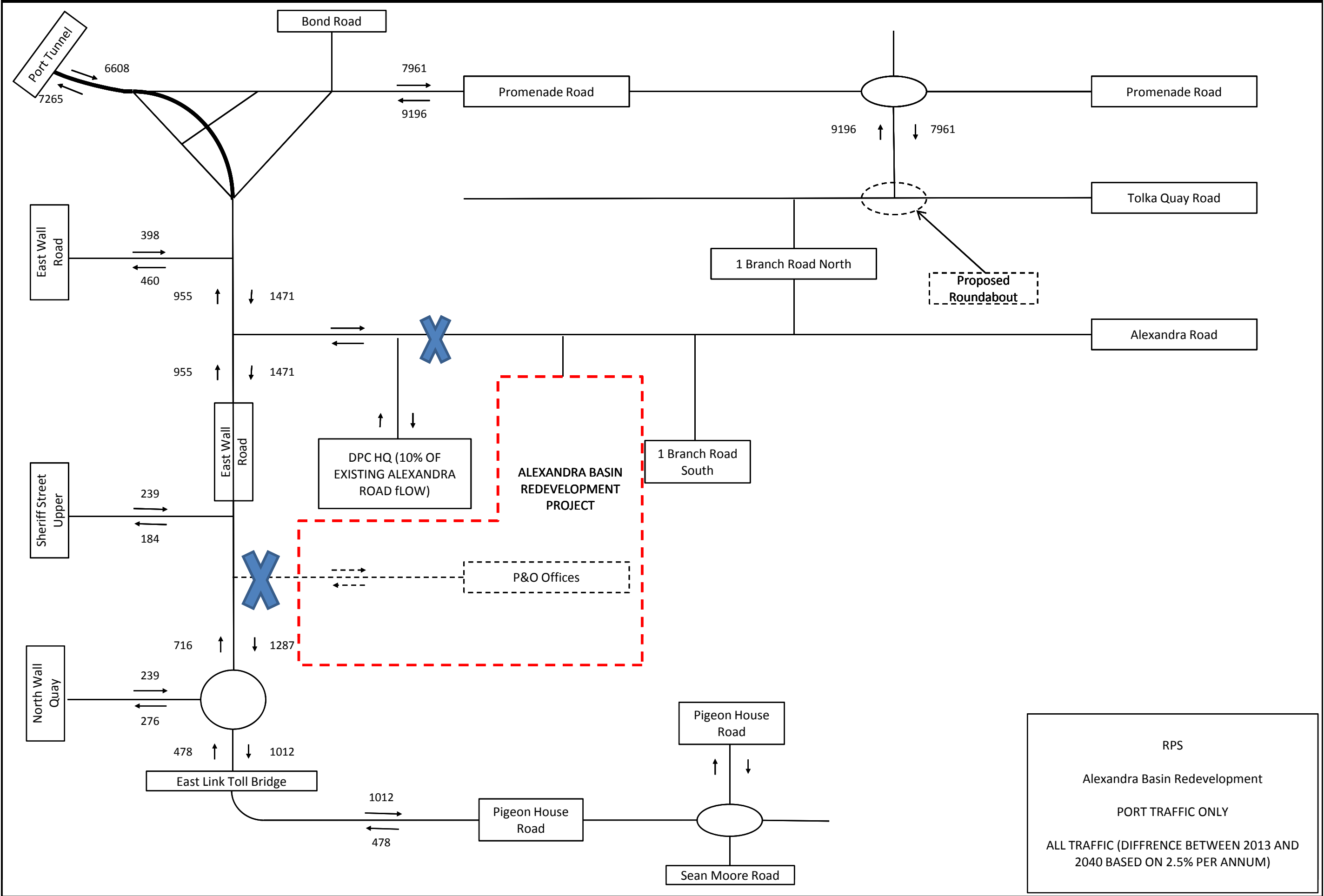




Increase in Port Traffic 2013 - 2034



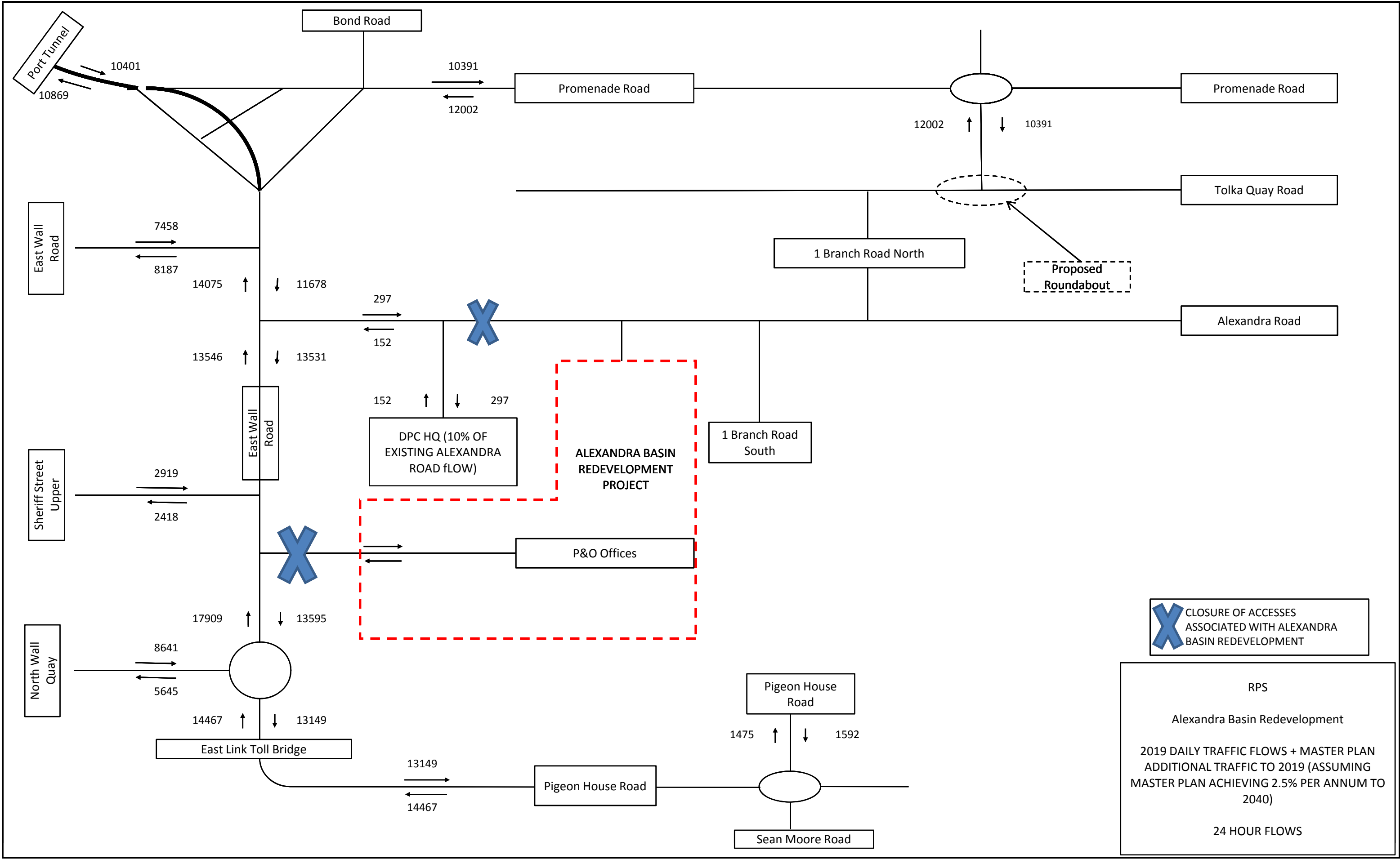
Increase in Port Traffic 2013 - 2040





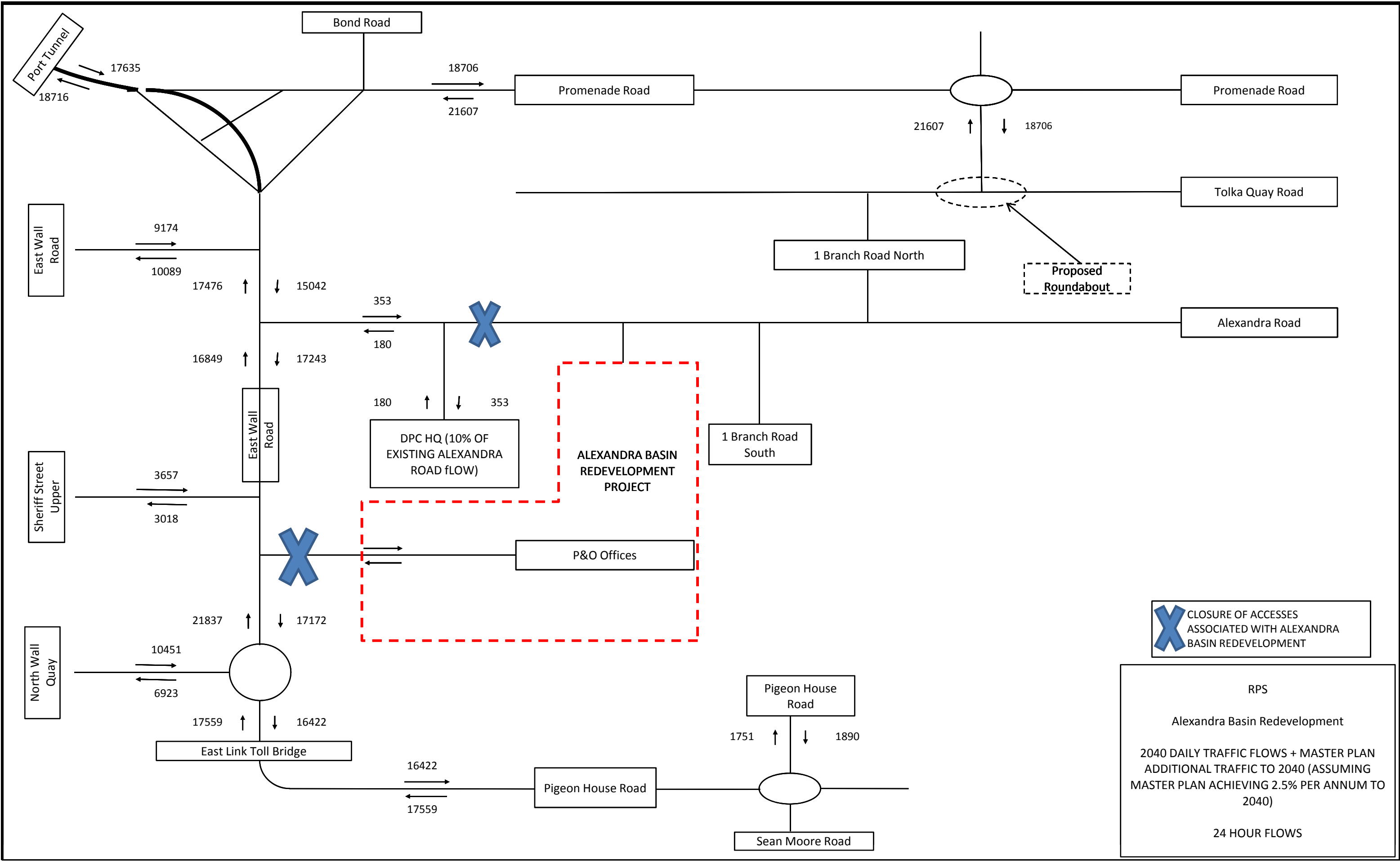
**Appendix 8.9 Impact of Increased Port Traffic Flows in Future Years**

2019 Proposed Daily Traffic Flows

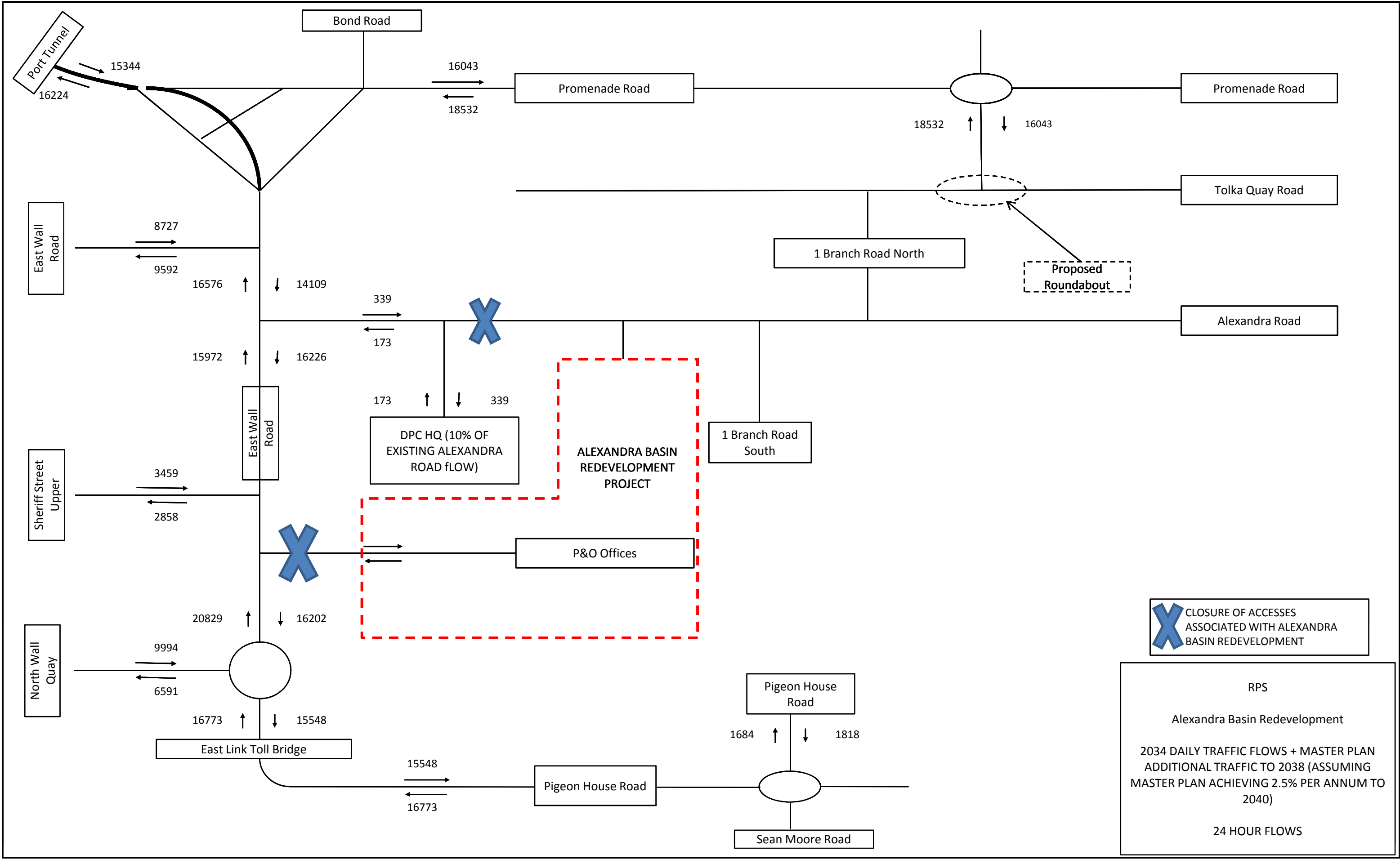




2024 Proposed Daily Traffic Flows



2034 Proposed Daily Traffic Flows



**X** CLOSURE OF ACCESSES  
ASSOCIATED WITH ALEXANDRA  
BASIN REDEVELOPMENT

RPS

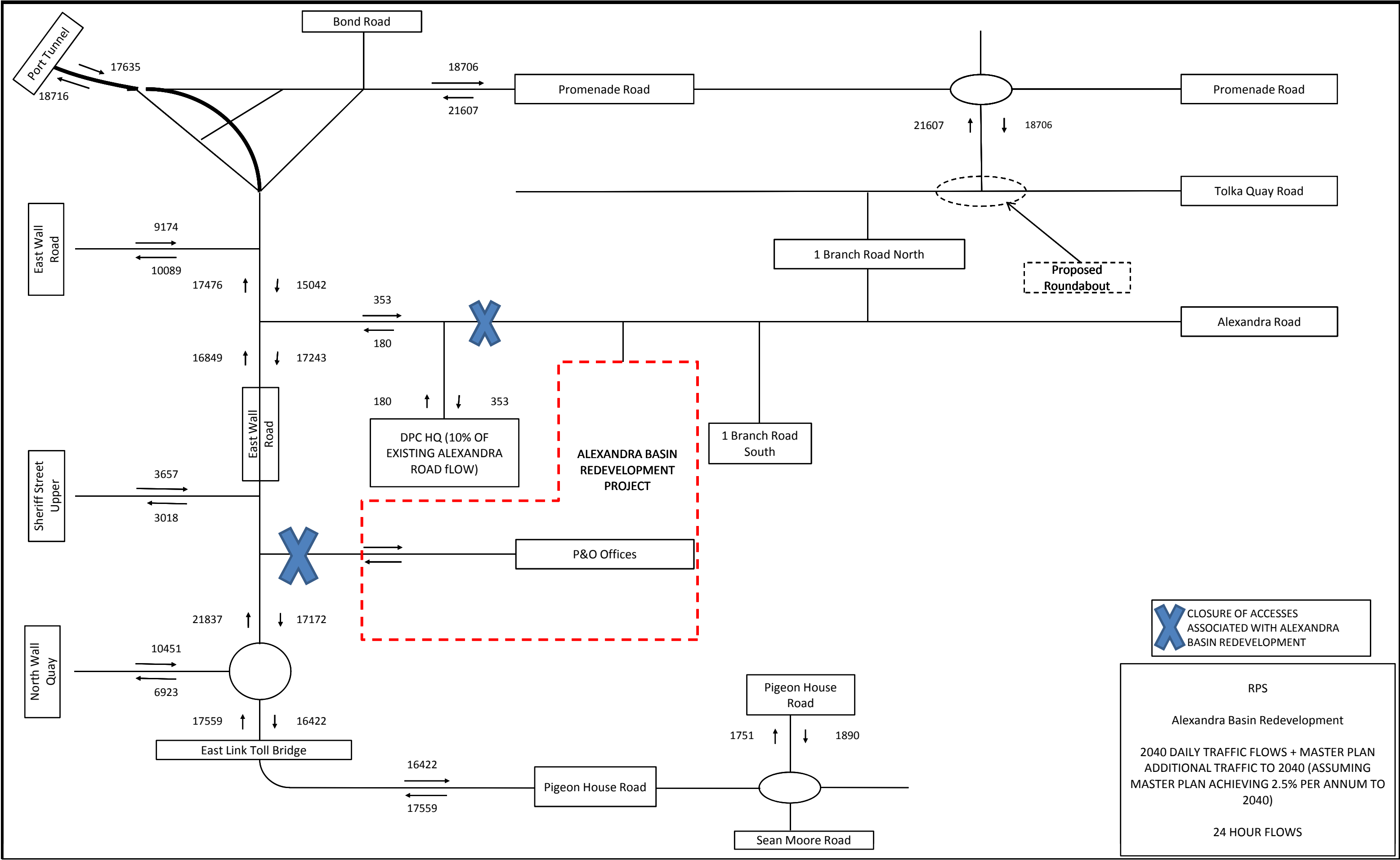
Alexandra Basin Redevelopment

2034 DAILY TRAFFIC FLOWS + MASTER PLAN  
ADDITIONAL TRAFFIC TO 2038 (ASSUMING  
MASTER PLAN ACHIEVING 2.5% PER ANNUM TO  
2040)

24 HOUR FLOWS



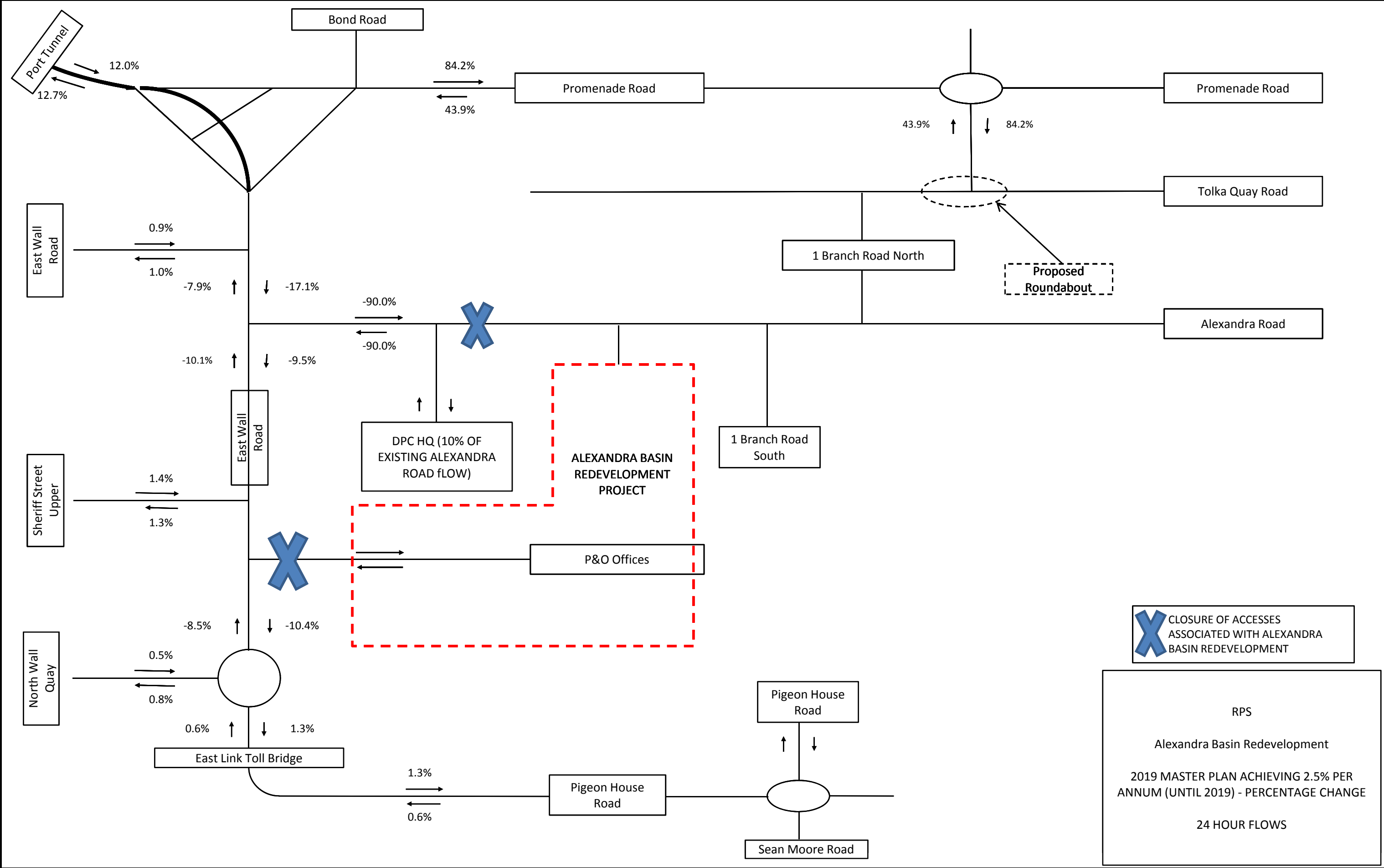
2040 Proposed Daily Traffic Flows



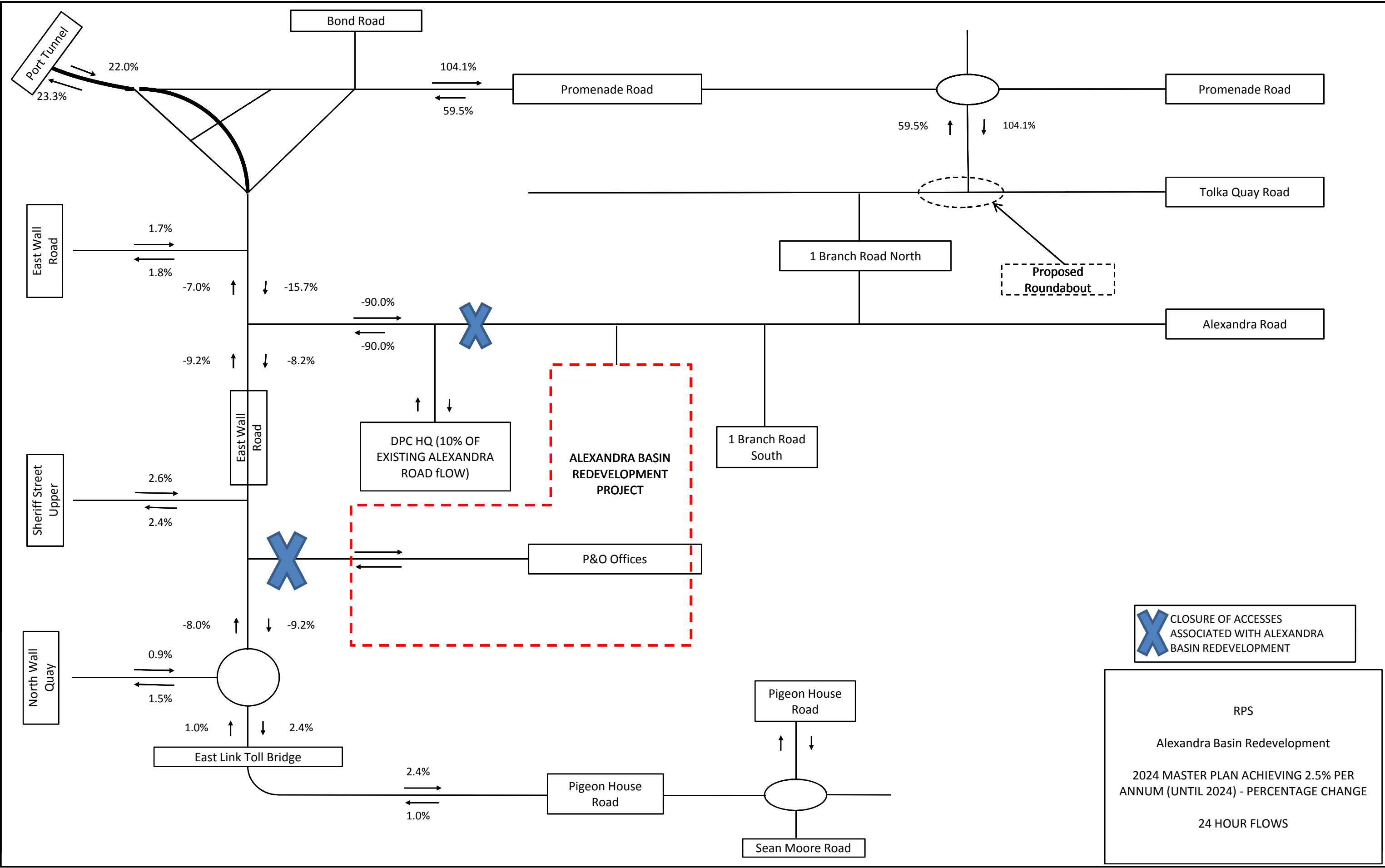
**Appendix 8.10 Impact of Increased Port Traffic Flows in Future Years as a Percentage**



2019 Percentage Impact



2024 Percentage Impact



**X** CLOSURE OF ACCESSES  
ASSOCIATED WITH ALEXANDRA  
BASIN REDEVELOPMENT

RPS

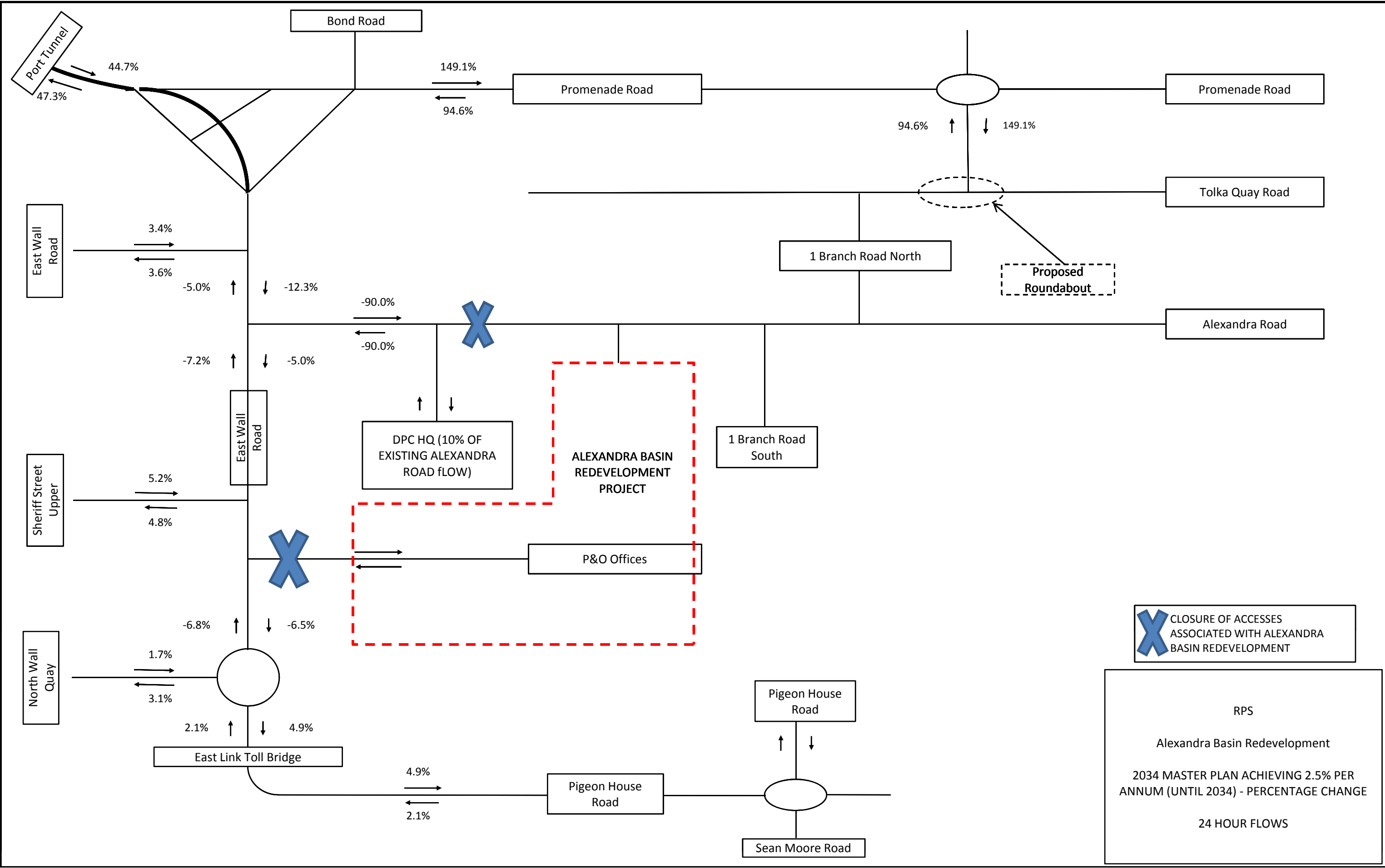
Alexandra Basin Redevelopment

2024 MASTER PLAN ACHIEVING 2.5% PER  
ANNUM (UNTIL 2024) - PERCENTAGE CHANGE

24 HOUR FLOWS



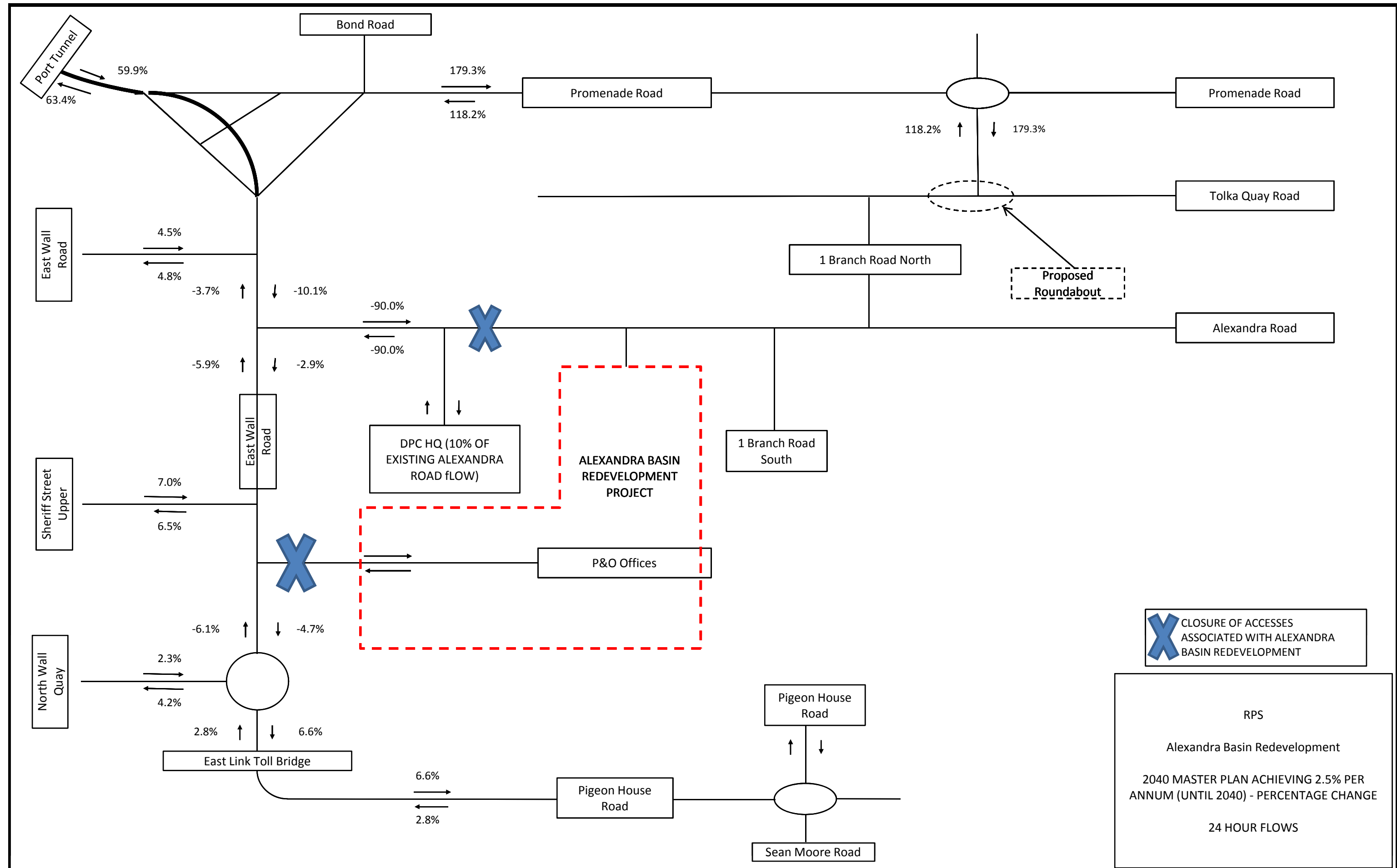
2034 Percentage Impact



**X** CLOSURE OF ACCESSES  
ASSOCIATED WITH ALEXANDRA  
BASIN REDEVELOPMENT

RPS  
Alexandra Basin Redevelopment  
2034 MASTER PLAN ACHIEVING 2.5% PER  
ANNUM (UNTIL 2034) - PERCENTAGE CHANGE  
24 HOUR FLOWS

## 2040 Percentage Impact





**This Environmental Impact Statement was prepared by:**

**RPS**  
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**On behalf of:**

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Dublin 1  
Telephone 01 887 6000  
Facsimile 01 836 5142  
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Web [www.dublinport.ie](http://www.dublinport.ie)



**The following sub-consultants carried out specialist studies:**

ADCO Ltd	Archaeology
Aquatic Services Unit	Fisheries and Benthic Ecology
Coastal and Marine Research Group	Marine Mammals
Natura Environmental Consultants	Birds
MacCabe Durney Barnes	Planning
Macroworks	Photomontages
Dr. Colin Rynne, UCC	Industrial Archaeological Heritage